



**STORAGE CAPACITIES**

EXISTING STORAGE			
'B' TYPE STORAGE	HOR 01	5,450 t	
'Q' TYPE STORAGE	HRC 01	60,000 t	
LOW STEEL FRAME OPEN BULKHEAD	(01)	20,000 t	
LOW CONCRETE FRAME OPEN BULKHEAD	(TBH 02)	25,000 t	
		<b>TOTAL EXISTING STORAGE</b>	<b>110,450 t</b>
PROPOSED STORAGE			
1.8m INT. STEEL FRAMED OPEN BULKHEAD	(TBH 03)	35,370 t	
		<b>TOTAL PROPOSED STORAGE</b>	<b>35,370 t</b>

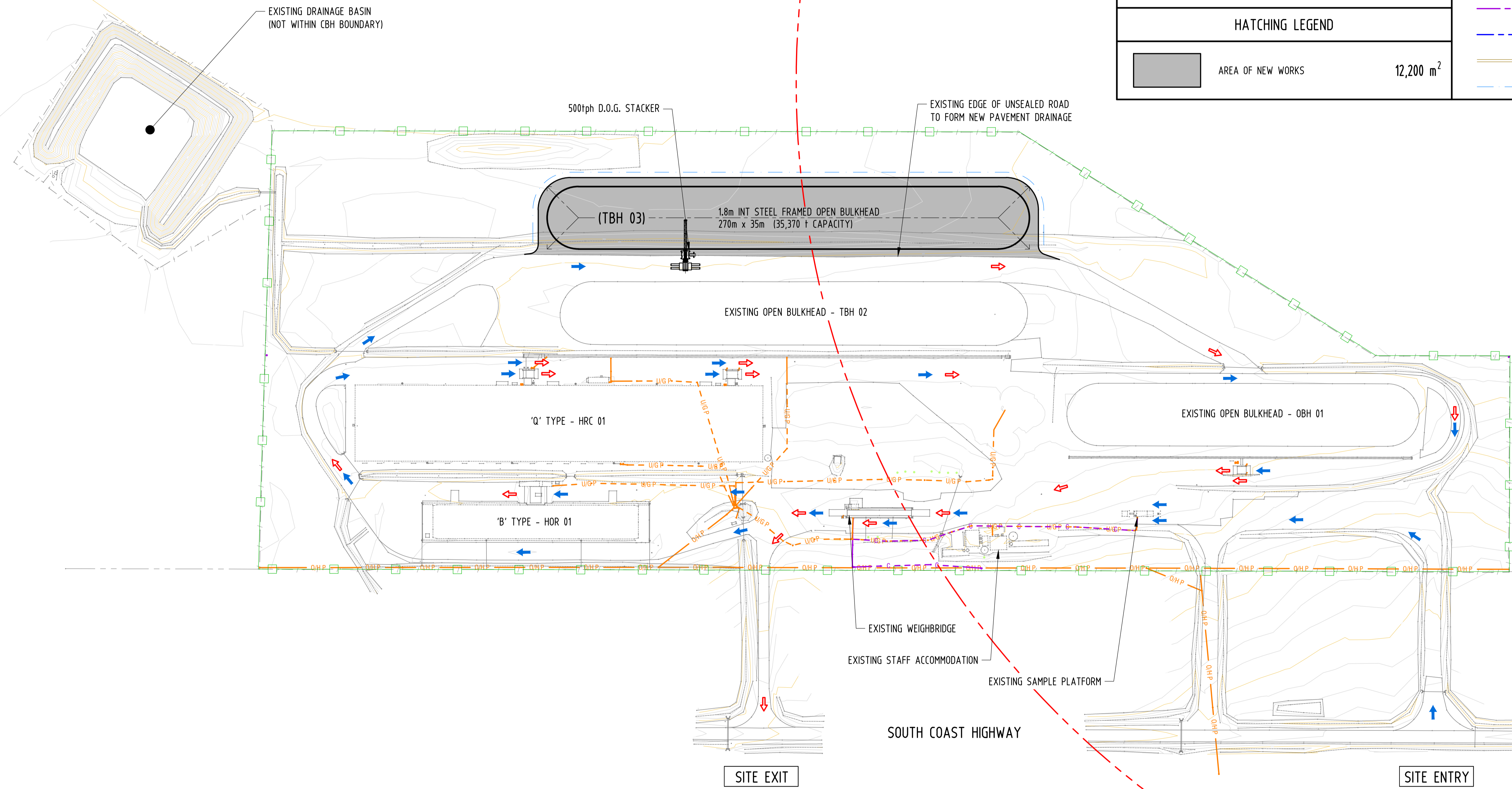
**TOTAL SITE STORAGE 145,820 t**

**HATCHING LEGEND**

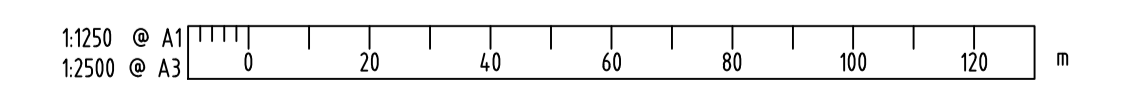
	AREA OF NEW WORKS	12,200 m <sup>2</sup>
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**DRAWING LEGEND**

	TRAFFIC MOVEMENTS - TRUCKS FULL
	TRAFFIC MOVEMENTS - TRUCKS EMPTY
	CADASTRAL BOUNDARY
	EPA INDUSTRIAL/SENSITIVE LAND USE SEPARATION DISTANCE - 500m RADIUS
	CBH SITE BOUNDARY
	CBH RAIL LEASE BOUNDARY
	OVERHEAD POWER LINES
	UNDERGROUND POWER CABLES
	UNDERGROUND COMMS CABLES
	UNDERGROUND WATER PIPES
	RAIL LINE
	PROPOSED OPEN DRAINS



**PRELIMINARY ISSUE**  
 DO NOT USE FOR CONSTRUCTION  
 DATE 18.06.21



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**CBH GROUP**  
 LEVEL 6  
 240 ST GEORGE'S TERRACE  
 PERTH W.A. 6000  
 PH (08) 9237 9600  
 FAX (08) 9322 3942

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		A	17.06.21	ISSUED AS CHECK PRINT AND FOR REVIEW	JB		

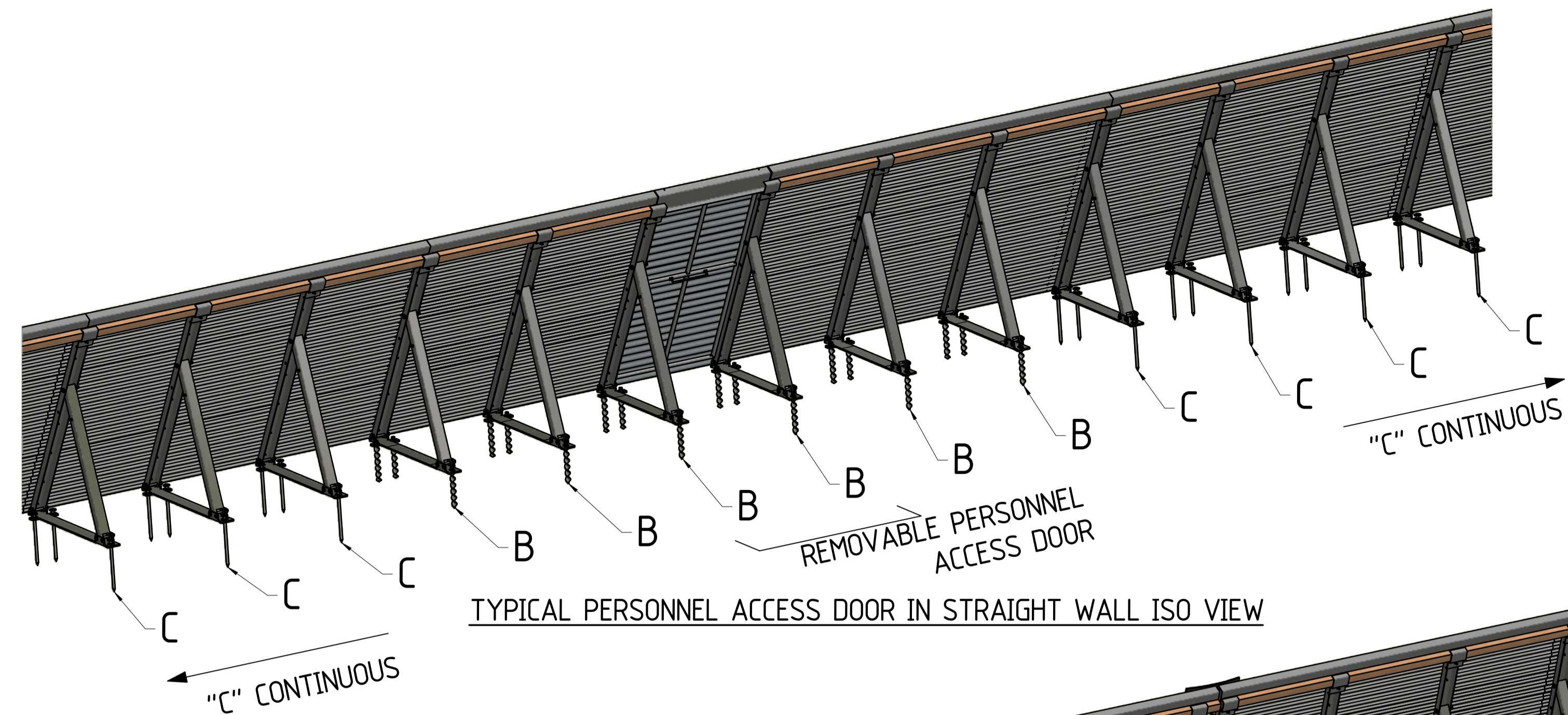
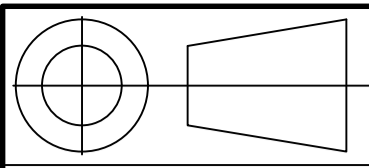
SCALE	1:1250	DRAWN	J. Butlingham	DATE	17.06.21
SHEET	A1	CHECKED			
PROJECT		DESIGNED			
CONTRACT No.		DESIGN APPR			
		PROJECT APPR			

TITLE	JACUP PRE FEASIBILITY STUDY 2021 EMERGENCY STORAGE CONCEPTUAL LAYOUT No. 1		
DRG No.	594-ENG-CI-DCO-0001	SHEET	1 OF 1
REV.			B

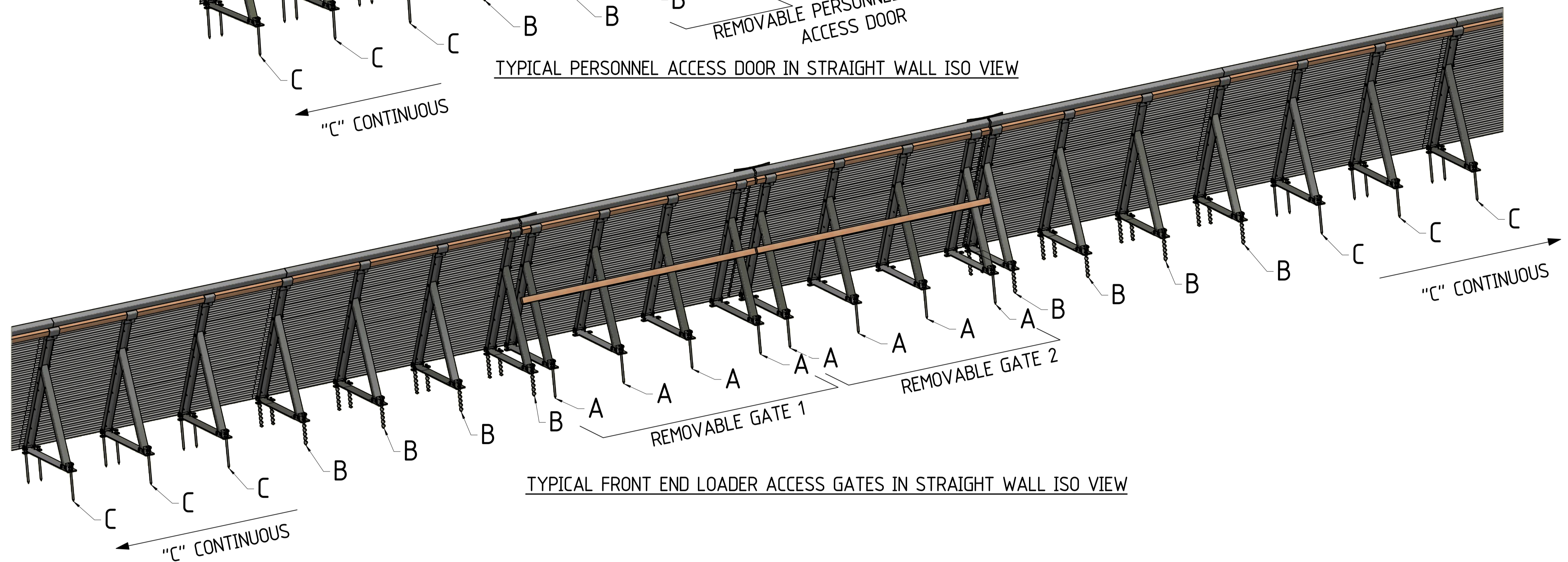




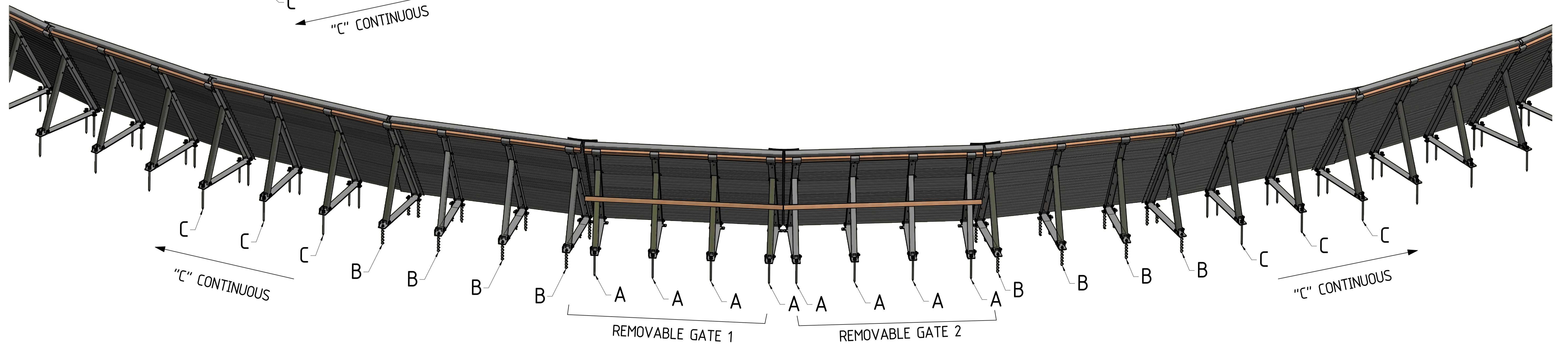




TYPICAL PERSONNEL ACCESS DOOR IN STRAIGHT WALL ISO VIEW



TYPICAL FRONT END LOADER ACCESS GATES IN STRAIGHT WALL ISO VIEW



TYPICAL FRONT END LOADER ACCESS GATES IN CURVED WALL ISO VIEW

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CBH GROUP HEAD OFFICE  
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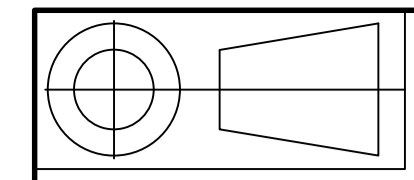
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CHECKED	LS 10/06/2020
ENGINEER	BC 11/06/2020
APPROVED	NH 11/06/2020

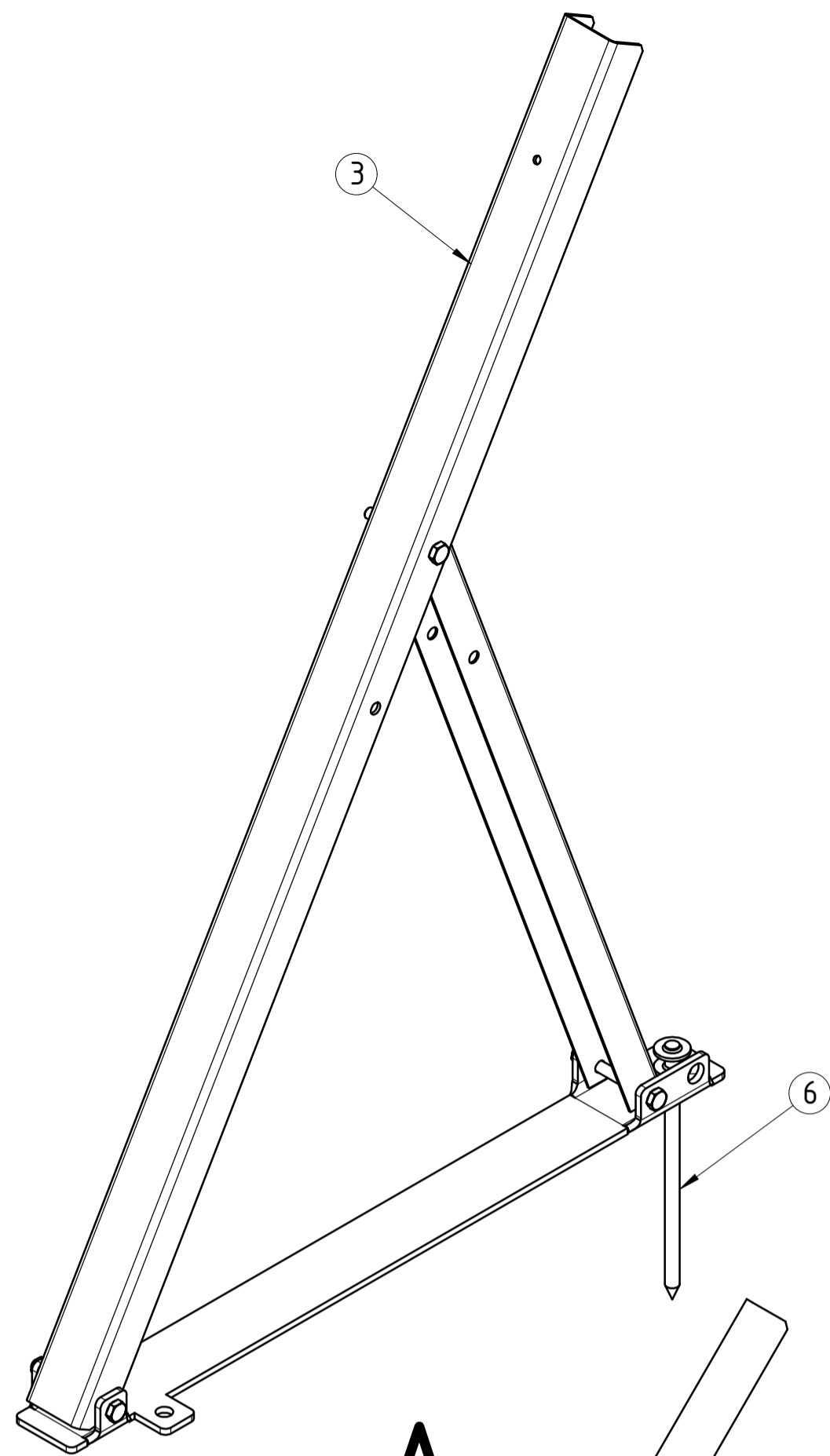
DRAWING TITLE  
1.8m OPEN BULK HEAD  
GENERAL ARRANGEMENT  
ANCHORINGS LAYOUT

SITE	VARIOUS	SIZE	A1
PROJECT	STANDARD		
DRAWING No	S119-ENG-ST-DGA-0003	SHEET	2 OF 9
REV.	0		

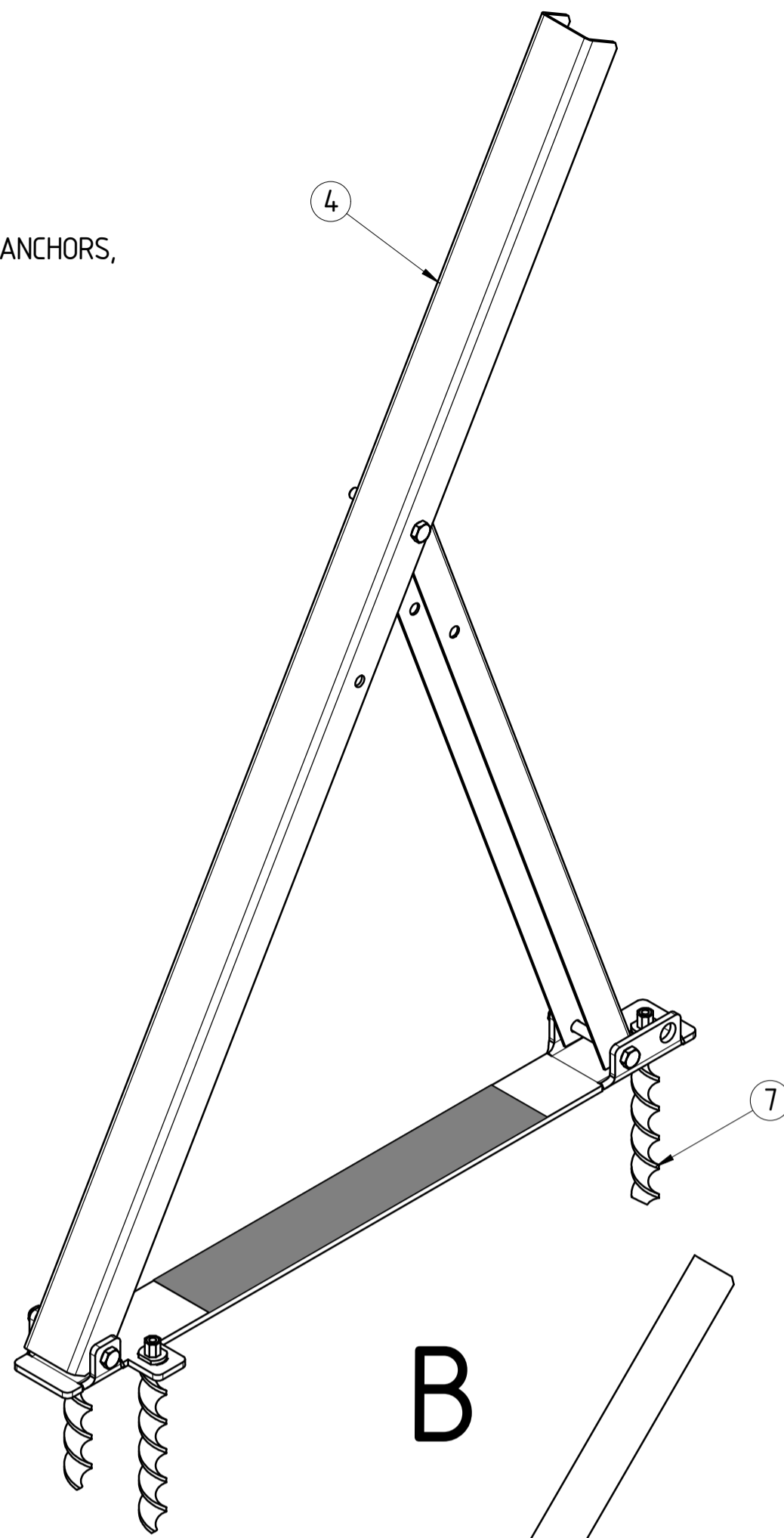




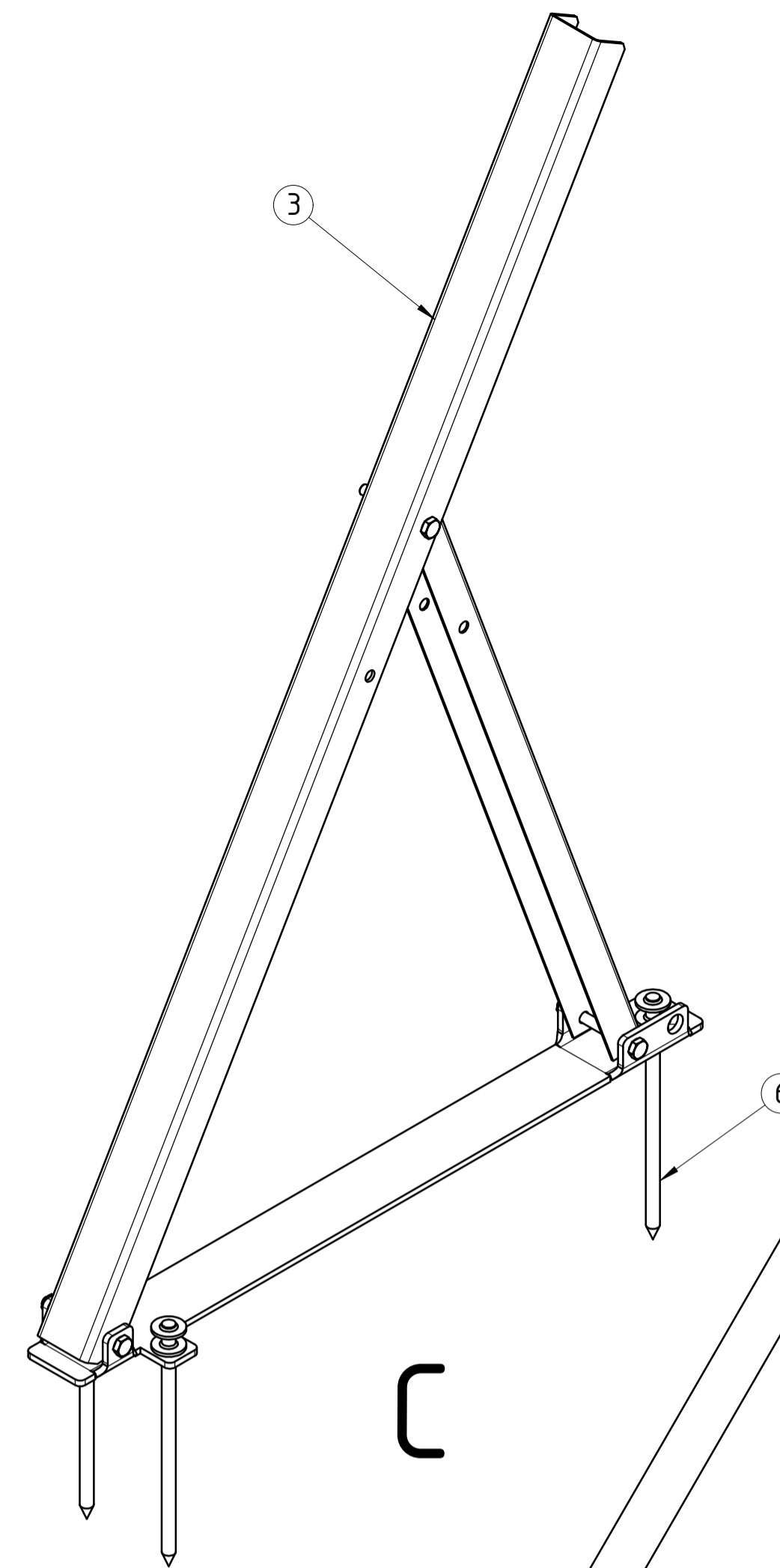
**NOTES:**  
 REMOVAL TOOL REQUIRED FOR SPIRIFIX ANCHORS,  
 M22 SPANNER/SOCKET,  
 HANDLE LENGTH TO SUIT CONDITIONS.



**A**



**B**



**C**

**ARRANGEMENT 'A' NOTES:**

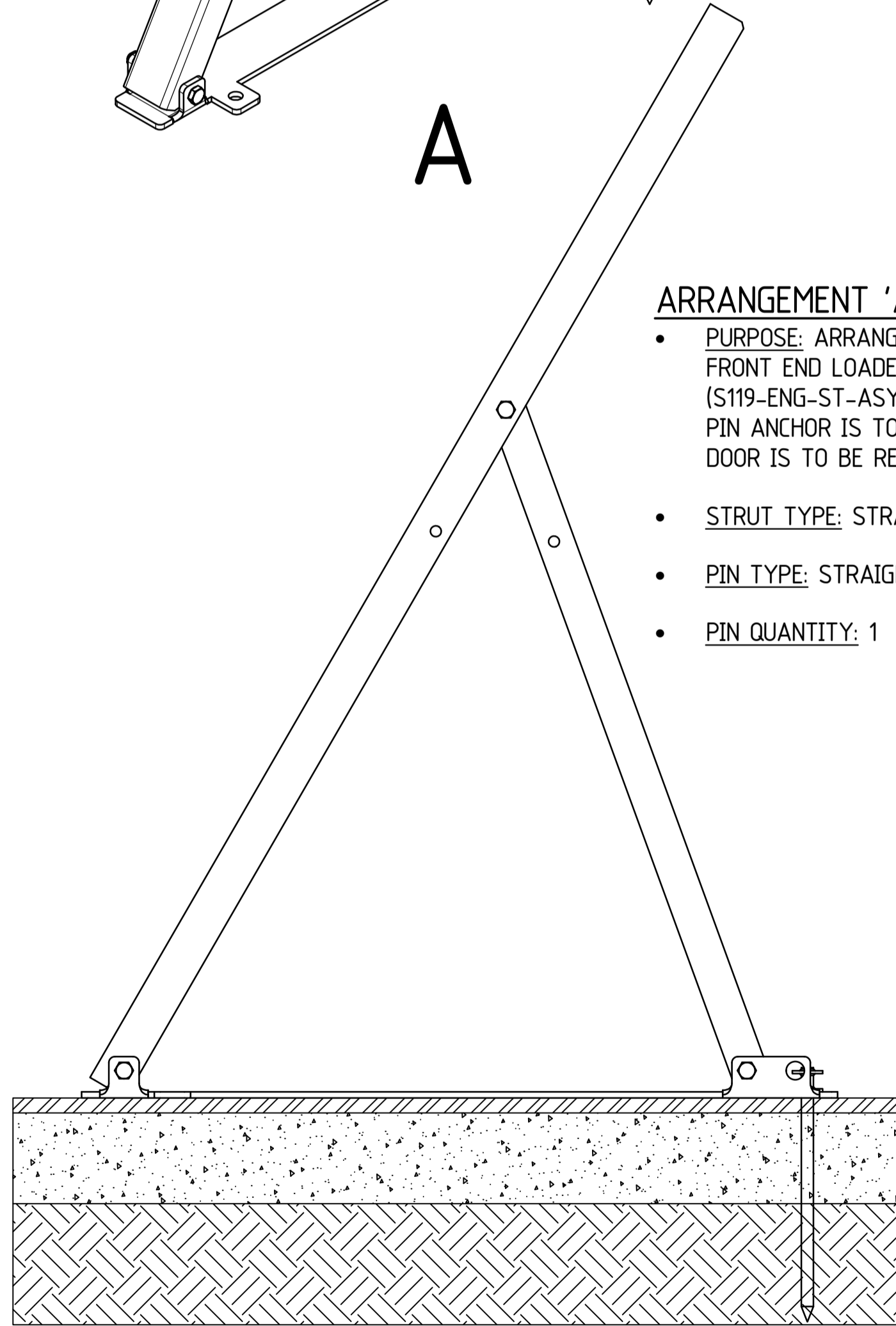
- PURPOSE: ARRANGEMENT 'A' IS USED IN THE FRONT END LOADER ACCESS GATE ASSEMBLY (S119-ENG-ST-ASY-0034). ONLY 1 STRAIGHT PIN ANCHOR IS TO BE INSTALLED AS THE DOOR IS TO BE REMOVABLE.
- STRUT TYPE: STRAIGHT PIN STRUT
- PIN TYPE: STRAIGHT PIN ANCHOR
- PIN QUANTITY: 1

**ARRANGEMENT 'B' NOTES:**

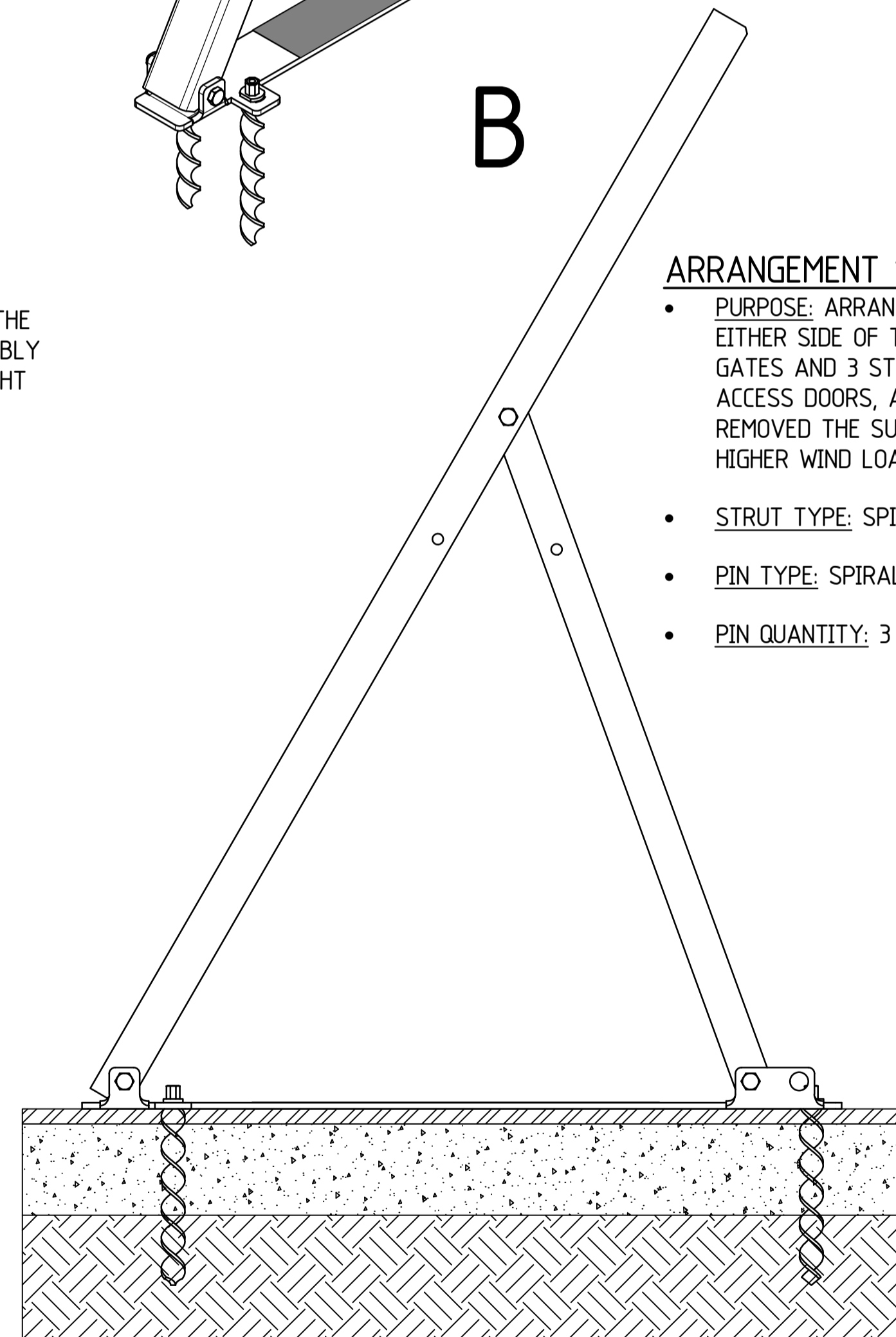
- PURPOSE: ARRANGEMENT 'B' IS USED 4 STRUTS EITHER SIDE OF THE FRONT END LOADER ACCESS GATES AND 3 STRUTS EITHER SIDE OF PERSONNEL ACCESS DOORS, AS WHEN THE GATES/DOORS ARE REMOVED THE SURROUNDING STRUTS EXPERIENCE HIGHER WIND LOADS.
- STRUT TYPE: SPIRAL PIN STRUT
- PIN TYPE: SPIRAL PIN ANCHOR
- PIN QUANTITY: 3

**ARRANGEMENT 'C' NOTES:**

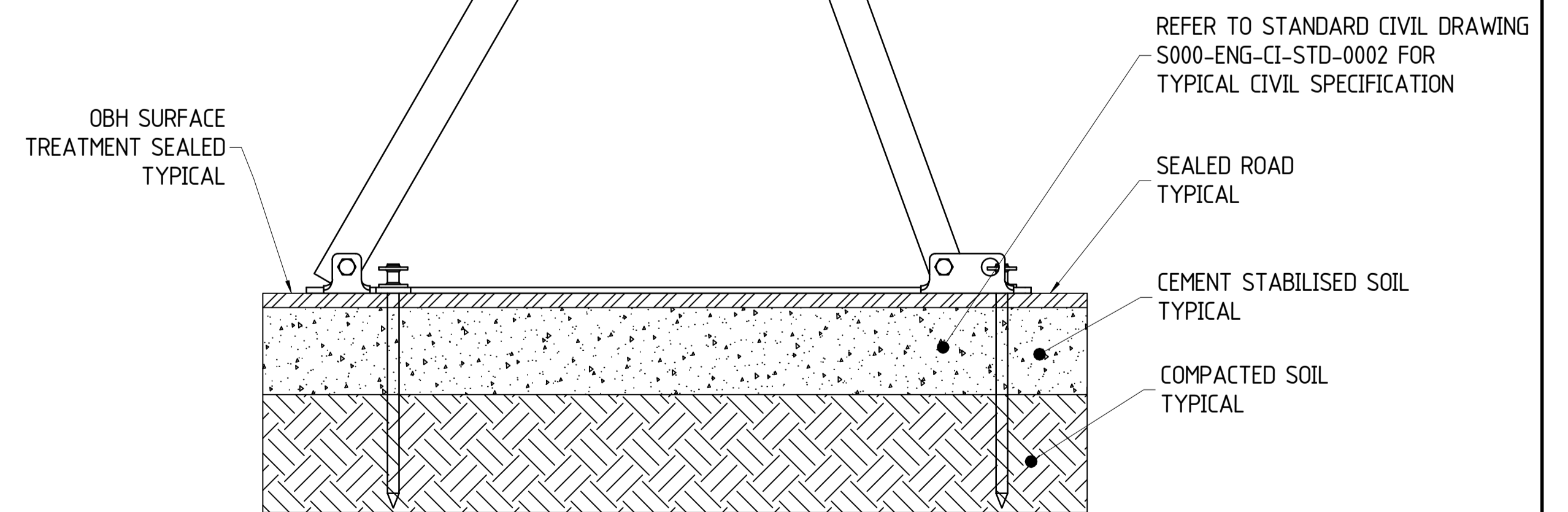
- PURPOSE: ARRANGEMENT 'C' IS USED IN ALL LOCATIONS, OTHER THAN WHERE ARRANGEMENT 'A' AND 'B' ARE USED
- STRUT TYPE: STRAIGHT PIN STRUT
- PIN TYPE: STRAIGHT PIN ANCHOR
- PIN QUANTITY: 3



**A**



**B**



**C**

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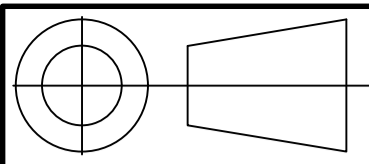
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DRAWN	SCR	10/06/2020
CHECKED	LS	10/06/2020
ENGINEER	BC	11/06/2020
APPROVED	NH	11/06/2020

DRAWING TITLE  
**1.8m OPEN BULK HEAD  
 GENERAL ARRANGEMENT  
 TYPICAL ANCHORINGS**

SITE	VARIOUS	SIZE	A1
PROJECT	STANDARD		
DRAWING No	S119-ENG-ST-DGA-0003	SHEET	3 OF 9
REV.			0





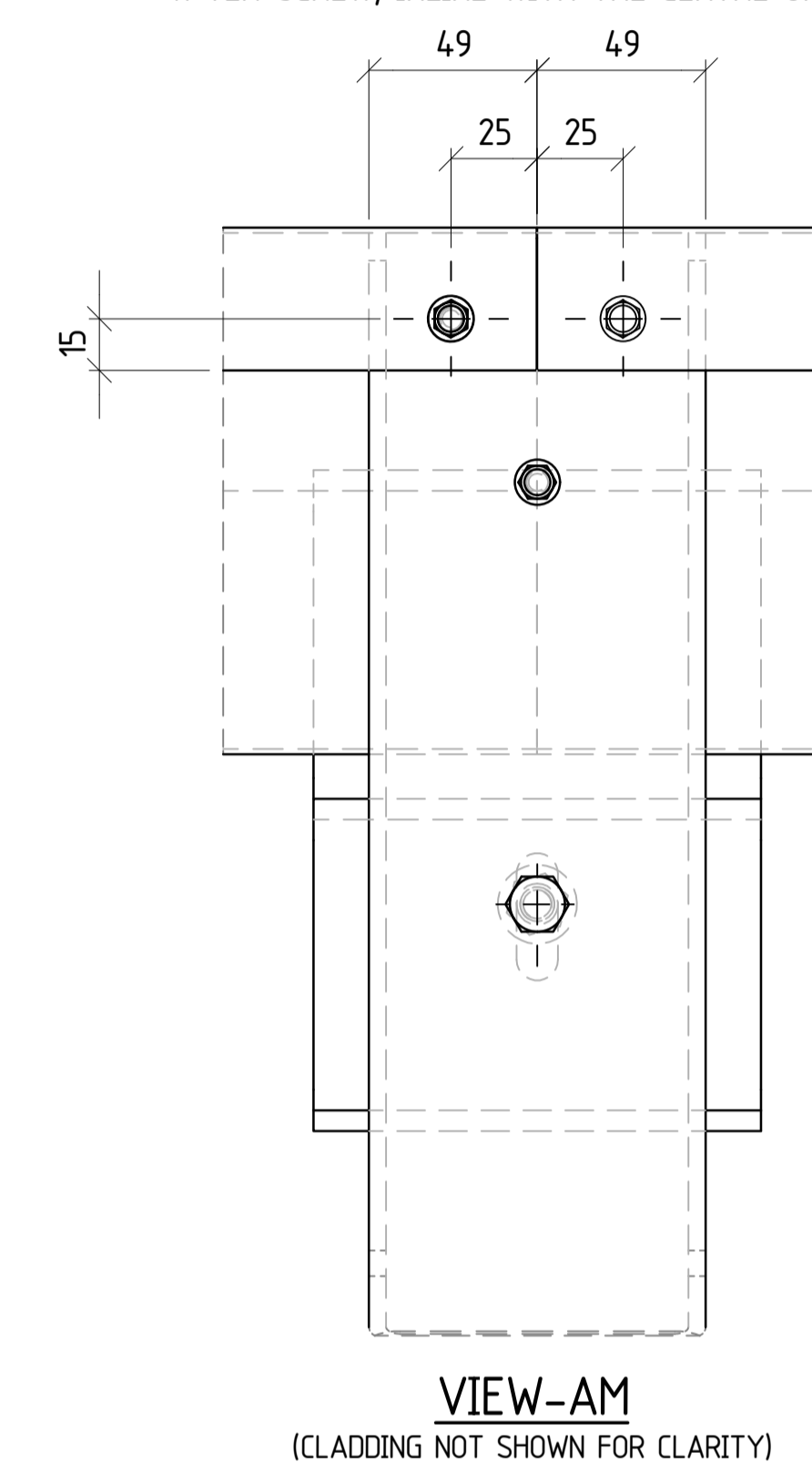
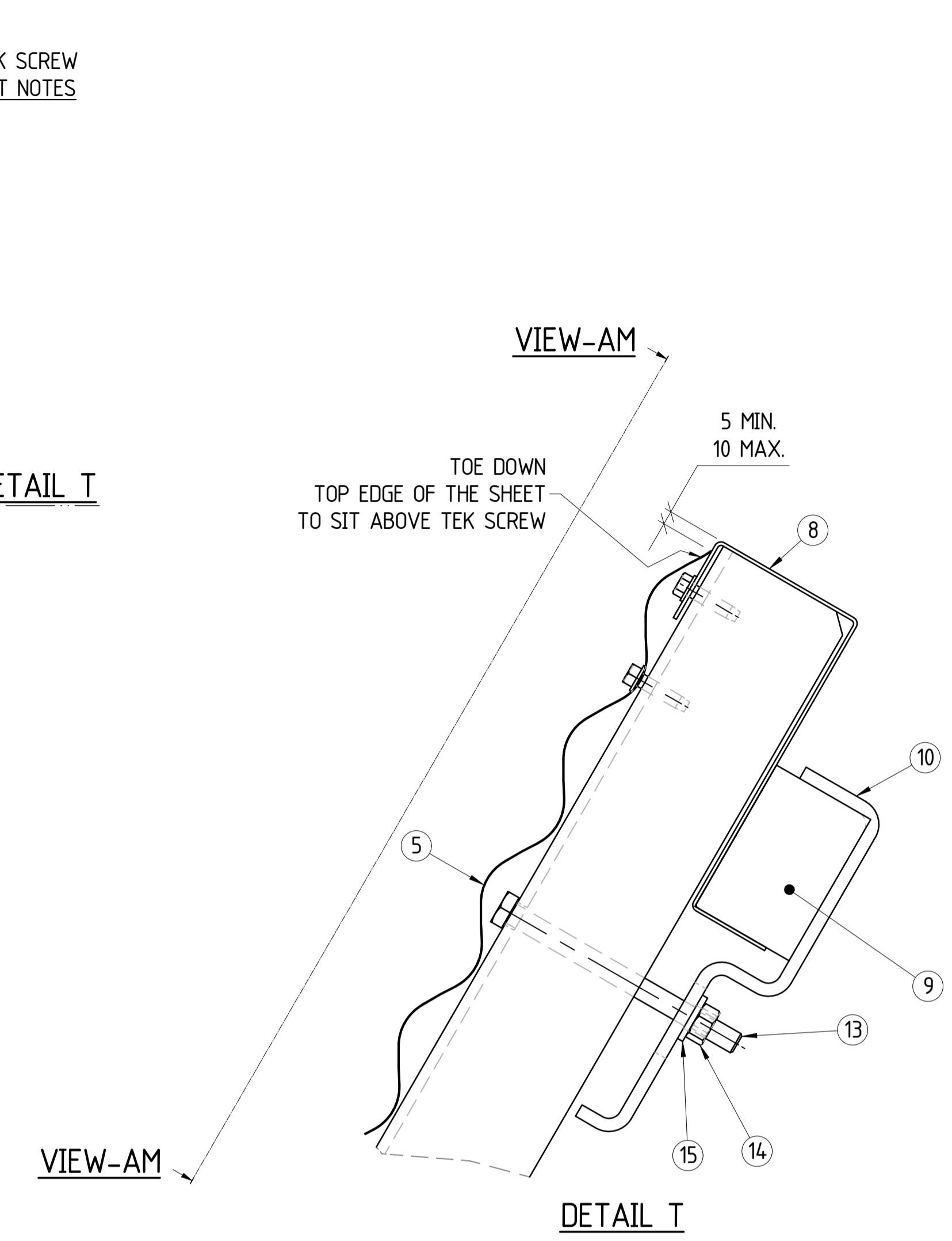
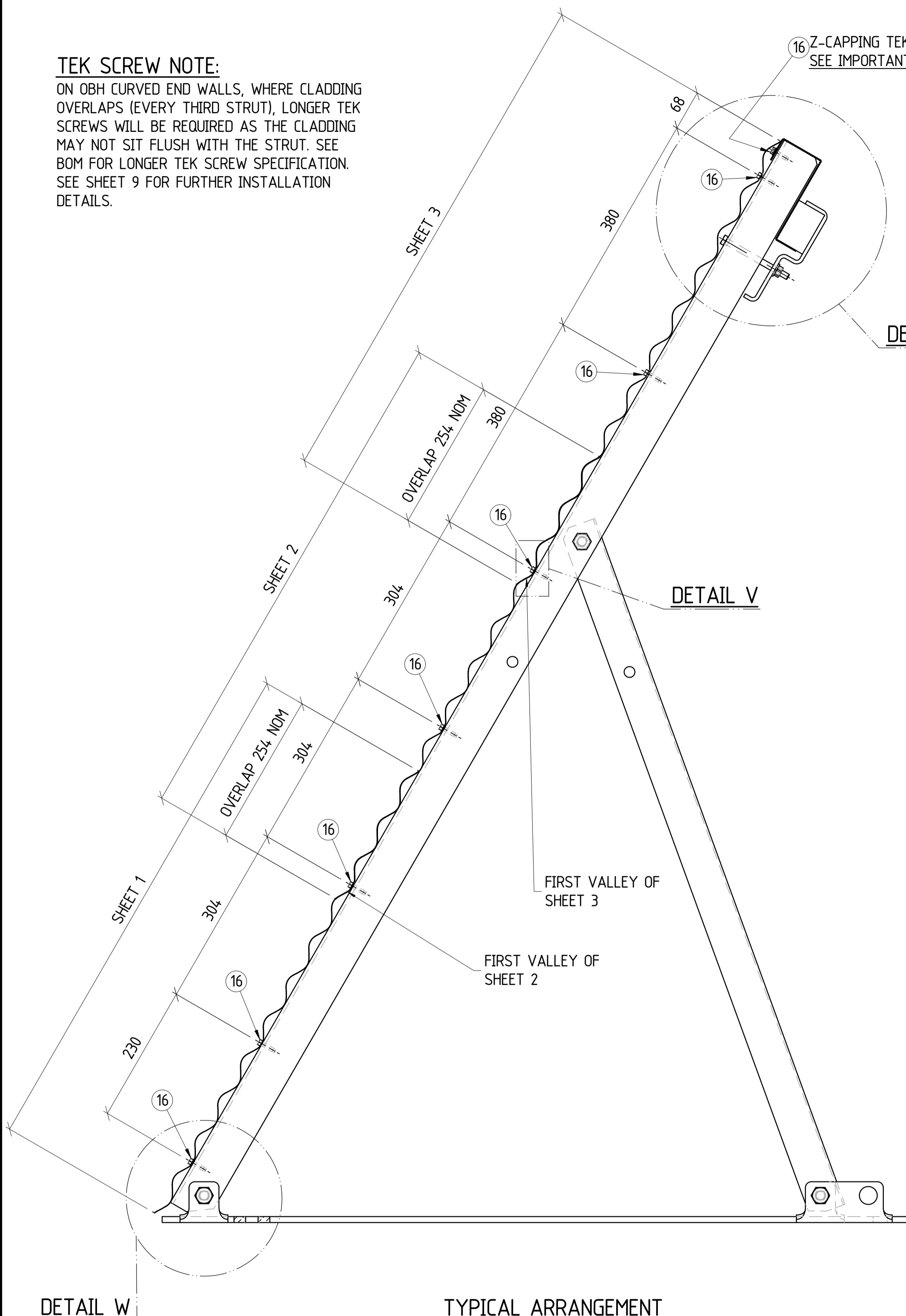
**TEK SCREW NOTE:**

ON OBH CURVED END WALLS, WHERE CLADDING OVERLAPS (EVERY THIRD STRUT), LONGER TEK SCREWS WILL BE REQUIRED AS THE CLADDING MAY NOT SIT FLUSH WITH THE STRUT. SEE BOM FOR LONGER TEK SCREW SPECIFICATION. SEE SHEET 9 FOR FURTHER INSTALLATION DETAILS.

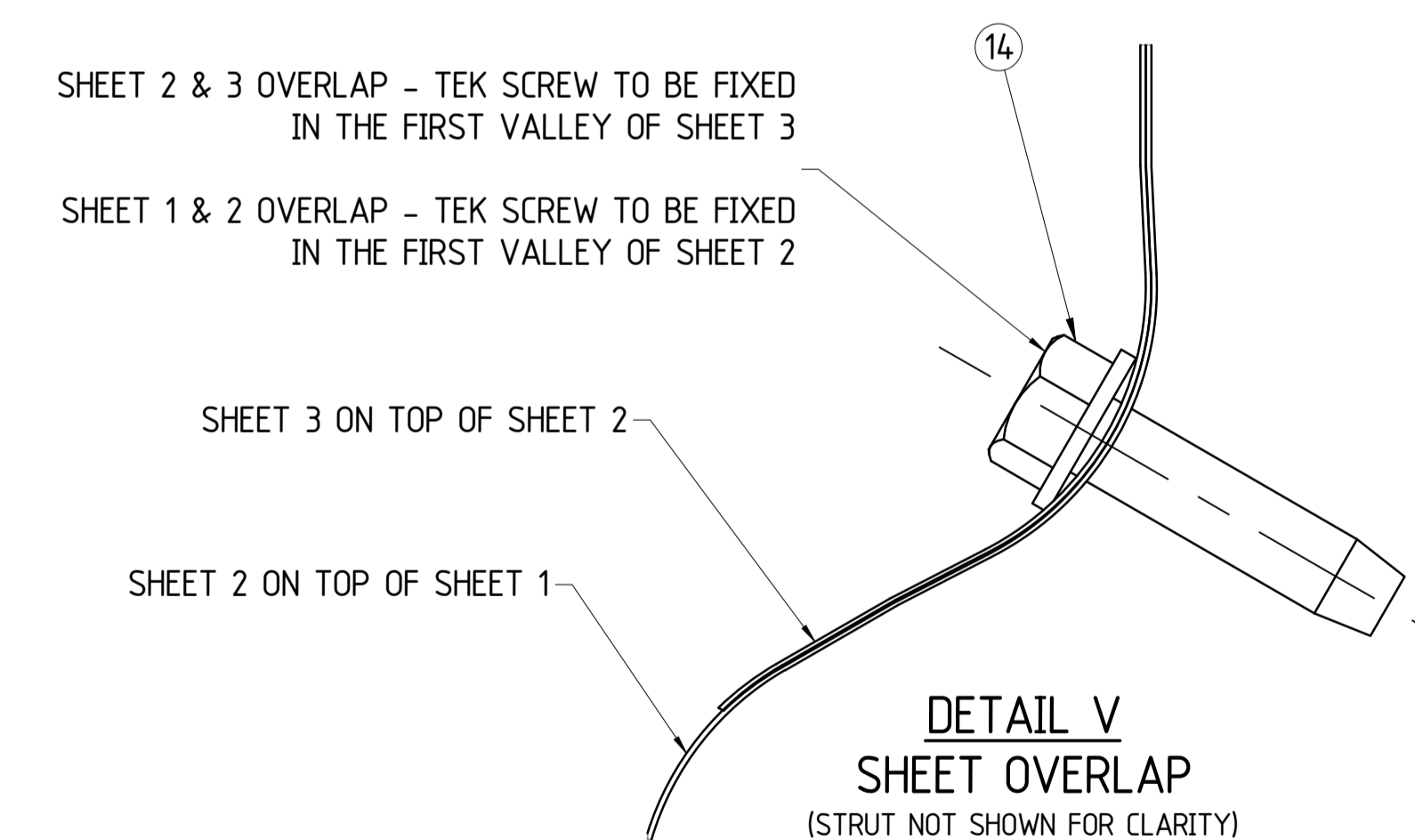
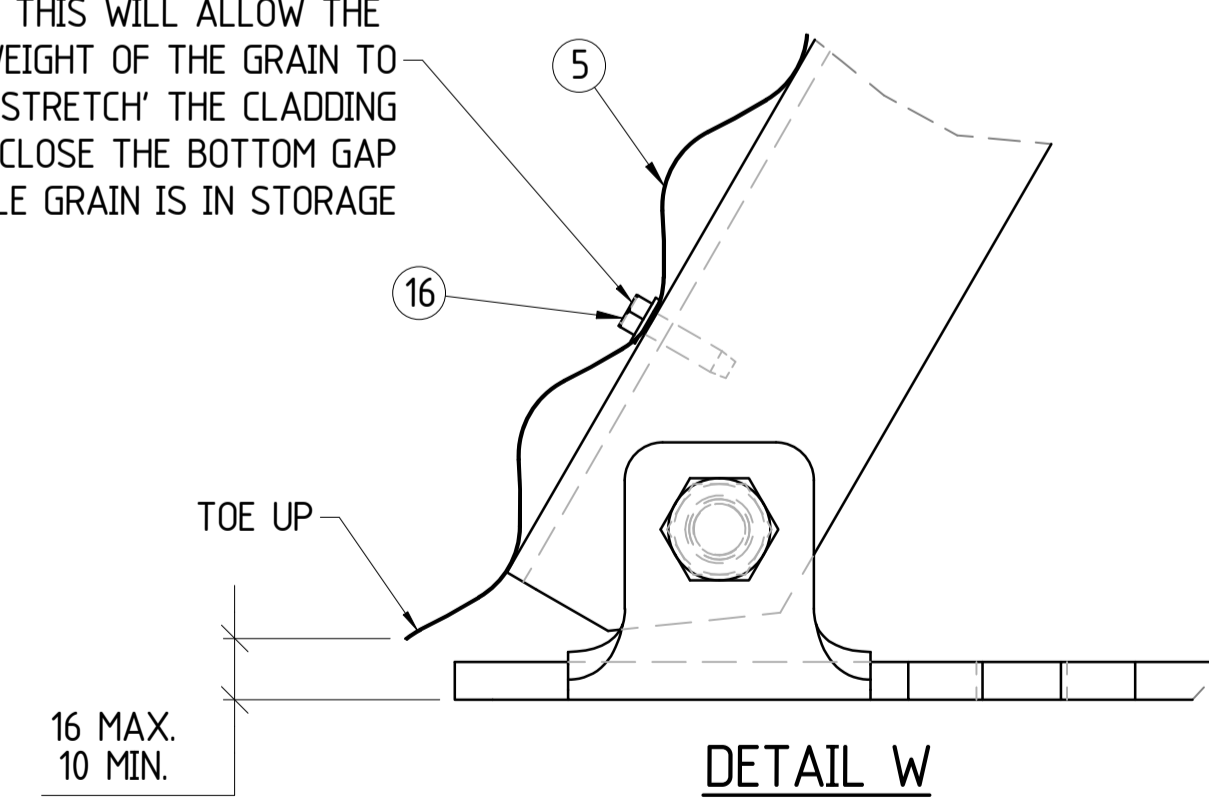
**IMPORTANT NOTE:**

TYPICAL ALL JOINTS, EXCEPT DOOR/GATE JOINTS:

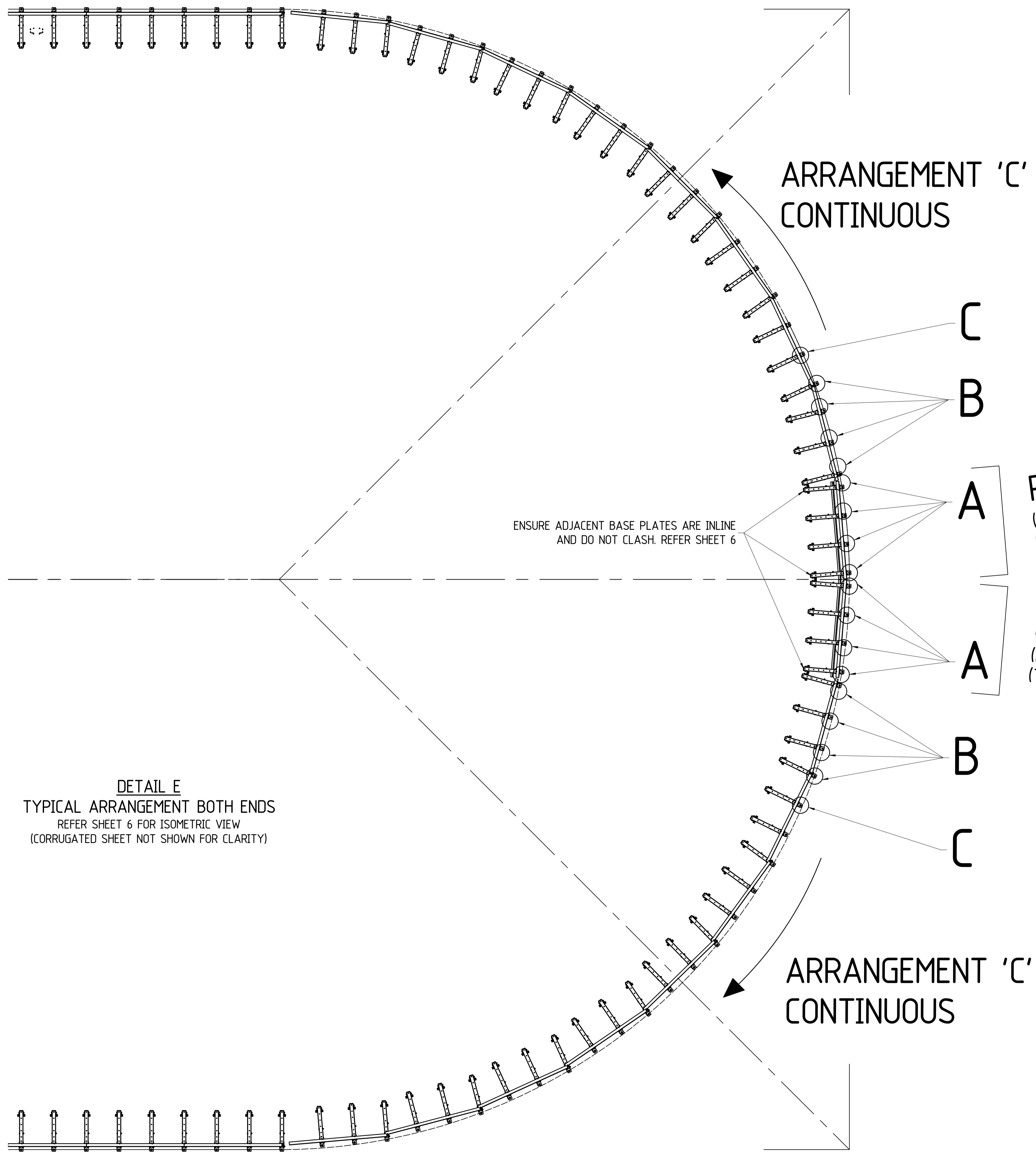
- 'Z' CAPPING SHALL BUTT JOIN TO THE CENTRE LINE OF THE STRUT.
- ALL Z CAPPING MUST BE ALIGNED AS ACCURATELY AS POSSIBLE. ANY MIS-ALIGNMENT IN THE 'Z' CAPPING WILL CREATE SHARP EDGES, WHICH MAY DAMAGE THE OBH TARP. MAXIMUM 'Z' CAPPING MISALIGNMENT TO BE 2mm IN ALL DIRECTIONS.
- WHERE 'Z' CAPPING BUTT JOINS OVER A STRUT, FIX 'Z' CAP WITH 2 x TEK SCREWS, ONE IN EACH 'Z' CAP (SHOWN BELOW)
- WHERE 'Z' CAPPING PASSES OVER A STRUT, FIX 'Z' CAP WITH 1 x TEK SCREW, INLINE WITH THE CENTRE OF THE STRUT.



PLACE THE BOTTOM TEK SCREW IN THE SECOND CORRUGATION AS THIS WILL ALLOW THE WEIGHT OF THE GRAIN TO 'STRETCH' THE CLADDING AND CLOSE THE BOTTOM GAP WHILE GRAIN IS IN STORAGE



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		DO NOT SCALE FROM THIS DRAWING		REF ORG No.	REFERENCE DRAWING TITLE	REV	DATE	REVISIONS	BY	CHK	APP	REV	DATE	REVISIONS	BY	CHK	APP	APPROVED	NH 11/06/2020	DRAWING No <b>S119-ENG-ST-DGA-0003</b>



DETAIL E  
TYPICAL ARRANGEMENT BOTH ENDS  
REFER SHEET 6 FOR ISOMETRIC VIEW  
(CORRUGATED SHEET NOT SHOWN FOR CLARITY)

ENSURE ADJACENT BASE PLATES ARE INLINE  
AND DO NOT CLASH. REFER SHEET 6

ARRANGEMENT 'C'  
CONTINUOUS

REMOVABLE GATE 2  
(REFER S119-ENG-ST-ASY-0034)  
(TAKE NOTE OF SHEET TRIMMING DETAIL ON CURVED WALL)

REMOVABLE GATE 1  
(REFER S119-ENG-ST-ASY-0034)  
(TAKE NOTE OF SHEET TRIMMING DETAIL ON CURVED WALL)

ARRANGEMENT 'C'  
CONTINUOUS

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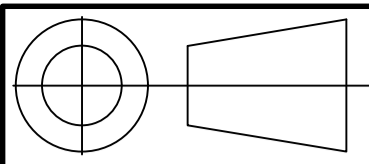
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CHECKED	LS	10/06/2020
ENGINEER	BC	11/06/2020
APPROVED	NH	11/06/2020

DRAWING TITLE  
1.8m OPEN BULK HEAD  
GENERAL ARRANGEMENT  
TYPICAL CURVED END WALL LAYOUT

SITE	VARIOUS	SIZE	A1
PROJECT	STANDARD	DRAWING No	S119-ENG-ST-DGA-0003
		SHEET	5 OF 9
		REV.	0

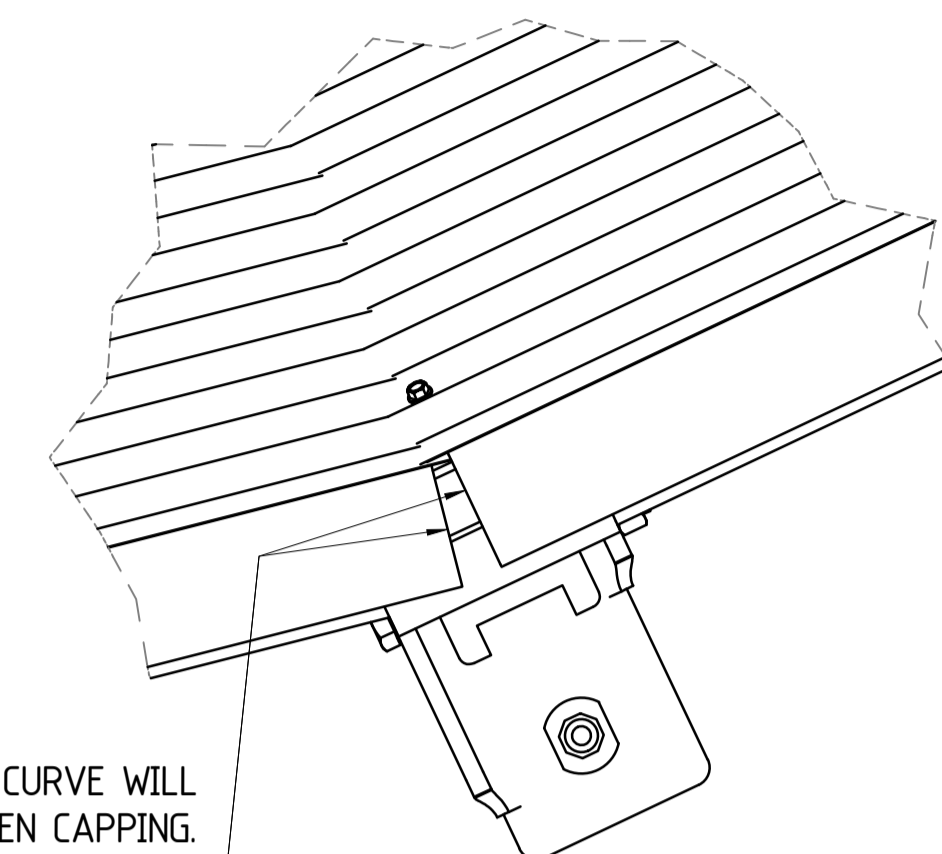
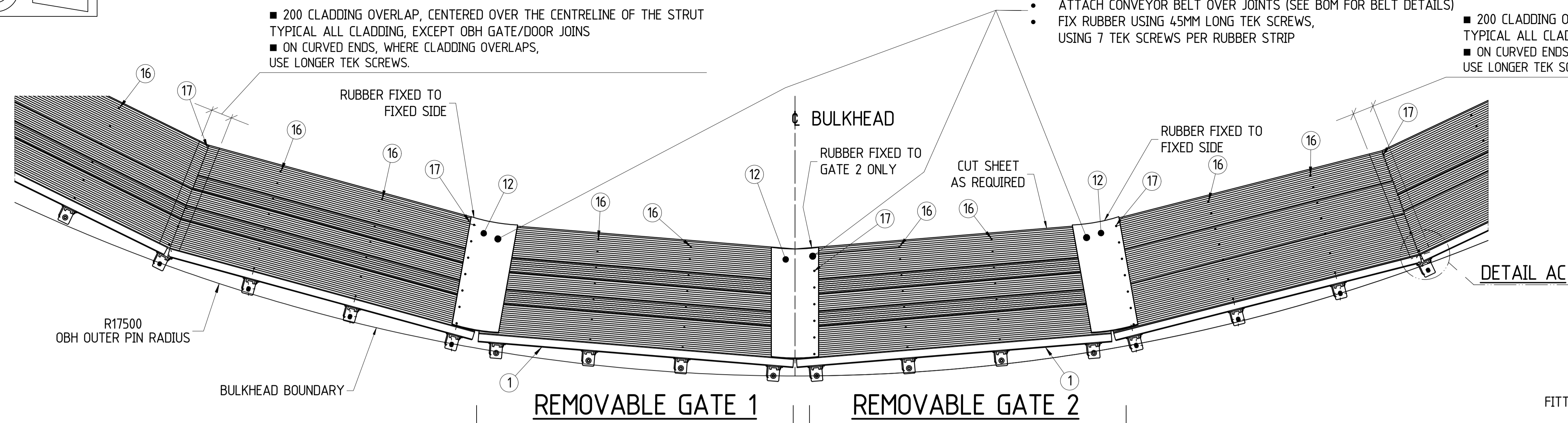




**TYPICAL RUBBER FIXINGS, BOTH ENDS**

- ATTACH CONVEYOR BELT OVER JOINTS (SEE BOM FOR BELT DETAILS)
- FIX RUBBER USING 45MM LONG TEK SCREWS, USING 7 TEK SCREWS PER RUBBER STRIP

- 200 CLADDING OVERLAP, CENTERED OVER THE CENTRELINE OF THE STRUT
- TYPICAL ALL CLADDING, EXCEPT OBH GATE/DOOR JOINS
- ON CURVED ENDS, WHERE CLADDING OVERLAPS, USE LONGER TEK SCREWS.



FITTING THE Z-CAPPING TO A CURVE WILL CREATE A GAP BETWEEN CAPPING. MINIMISE GAP AS MUCH AS POSSIBLE TO AVOID THE SHARP EDGES RIPPING THE OBH TARP. VERTICAL AND HORIZONTAL MISALIGNMENT SHOULD BE A MAXIMUM OF 2MM.

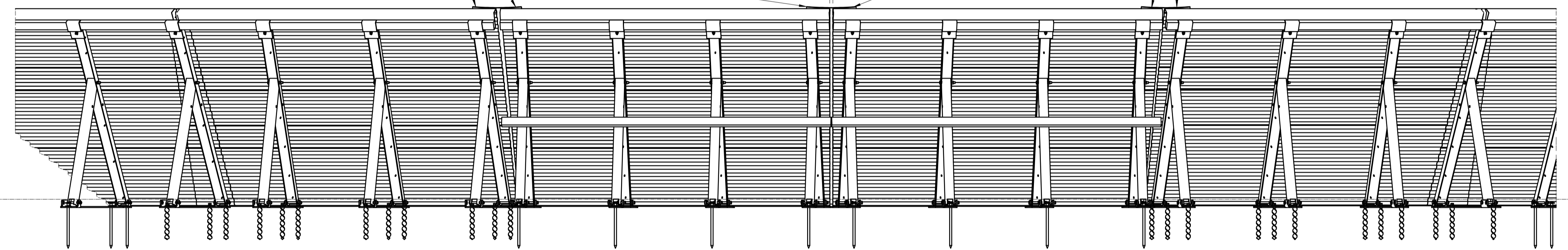
**DETAIL AC**

CLADDING, Z-CAPPING AND WOOD, ALL TRIMMED TO SUIT ON BOTH SIDES TO CREATE A BUTT JOIN BETWEEN THE FIXED WALL AND THE REMOVABLE GATE. REFER S119-ENG-ST-ASY-0003 FOR CLADDING TRIMMING DETAIL. MAX ALLOWABLE GAP BETWEEN BUTT JOINTS IS 20mm - TYPICAL ALL GATES

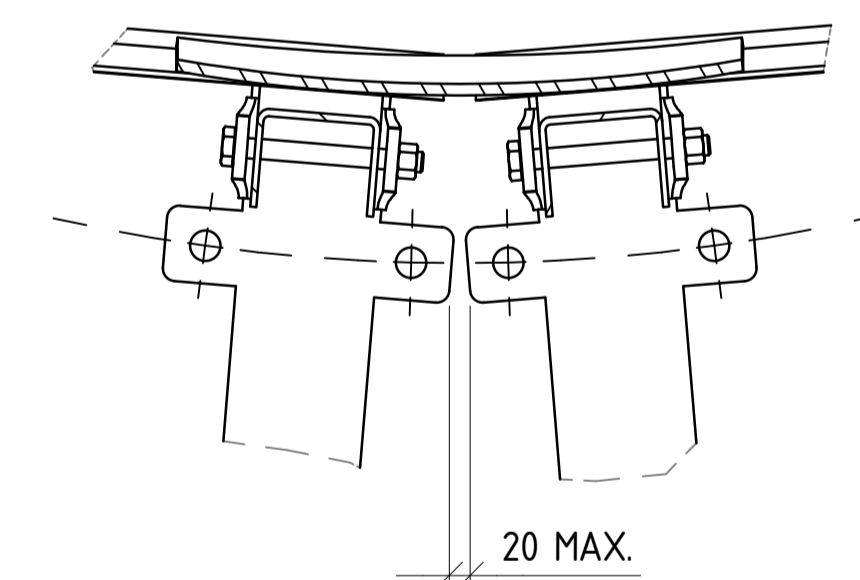
**DETAIL A**  
FRONT END LOADER ACCESS GATES IN CURVED WALL  
TYPICAL BOTH ENDS

CLADDING, Z-CAPPING AND WOOD, ALL TRIMMED TO SUIT ON BOTH SIDES TO CREATE A BUTT JOIN BETWEEN THE FIXED WALL AND THE REMOVABLE GATE. REFER S119-ENG-ST-ASY-0003 FOR CLADDING TRIMMING DETAIL. MAX ALLOWABLE GAP BETWEEN BUTT JOINTS IS 20mm - TYPICAL ALL GATES

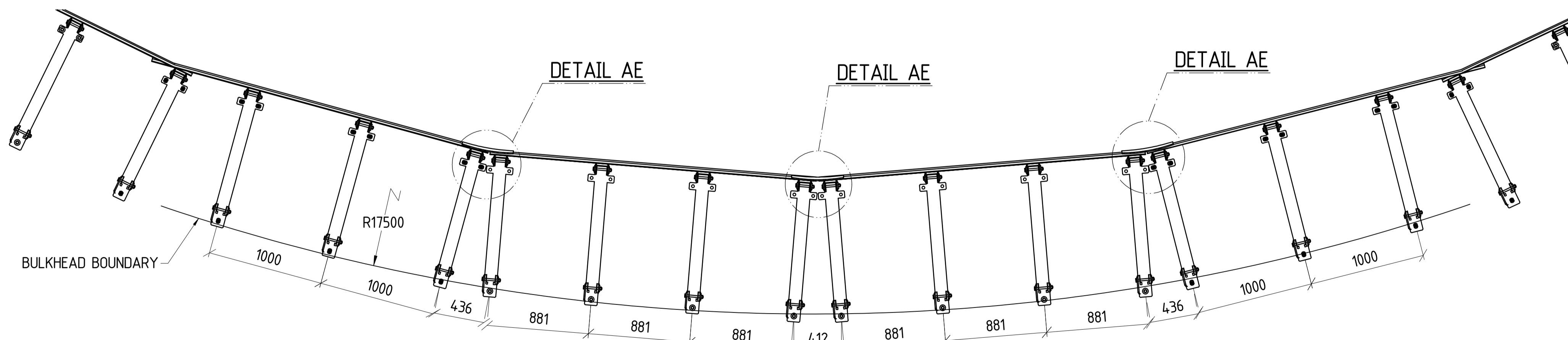
20 MAX. GAP  
TYPICAL ALL ACCESS WAYS



**ELEVATION**



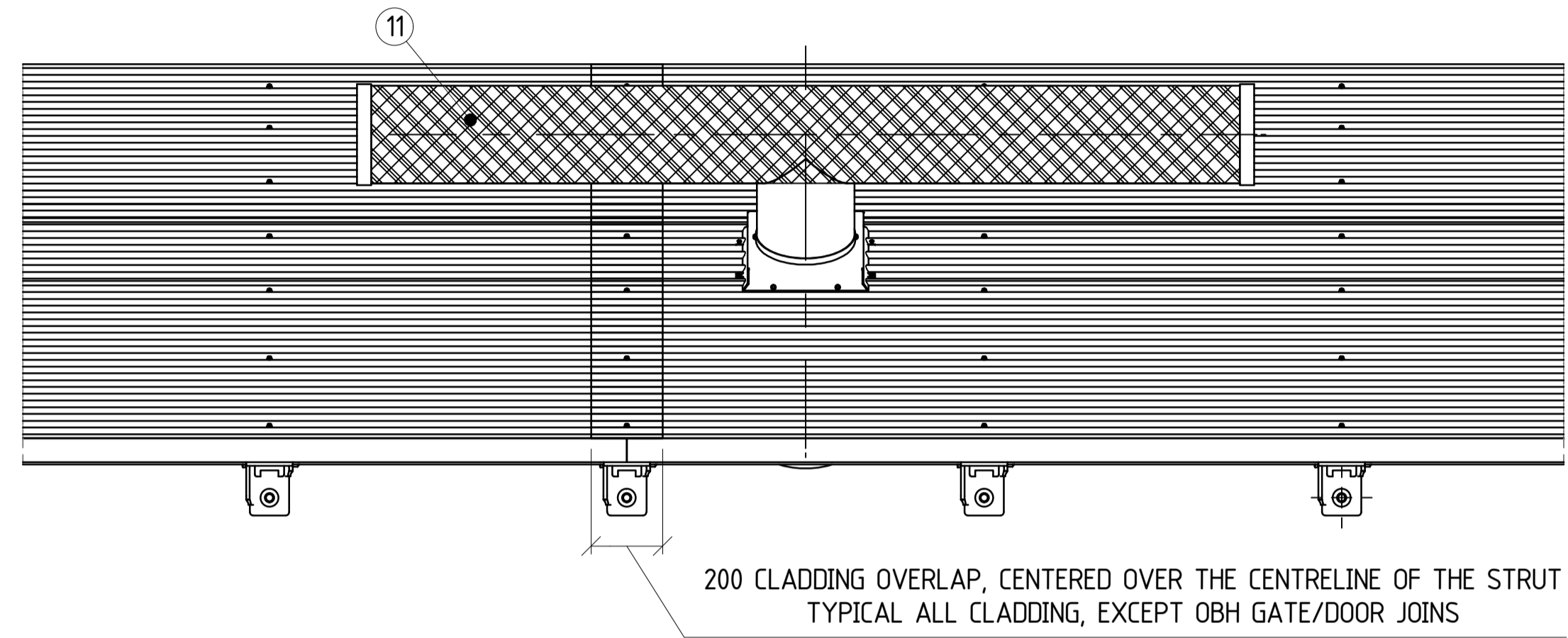
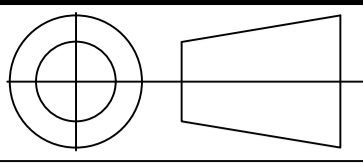
**DETAIL AE**  
TYPICAL STRUT FOOT ALIGNMENT AT GATE JOINS  
(PINS NOT SHOWN FOR CLARITY)



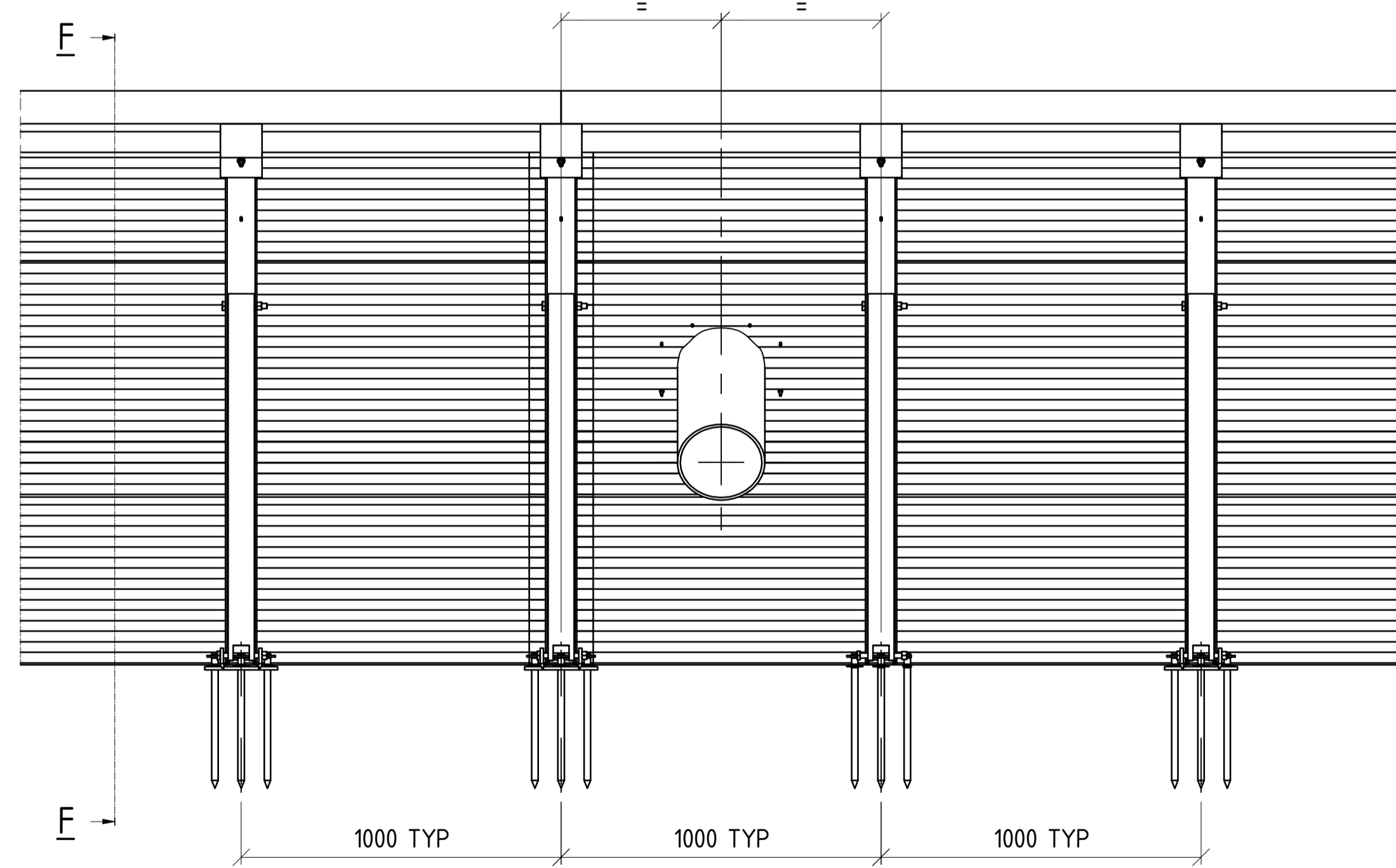
**SECTION B**  
FRAMES FOOT PRINT

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		DO NOT SCALE FROM THIS DRAWING												PROJECT <b>STANDARD</b>		DRAWING No <b>S119-ENG-ST-DGA-0003</b>		SHEET <b>6 OF 9</b>		REV. <b>0</b>
		REF DRG No.	REFERENCE DRAWING TITLE	REV	DATE	REVISIONS	BY	CHK	APP	REV	DATE	REVISIONS	BY	CHK	APP	APPROVED	0 10/06/2020 COPIED FROM S-119, ISSUED FOR CONSTRUCTION SCR LS NH			

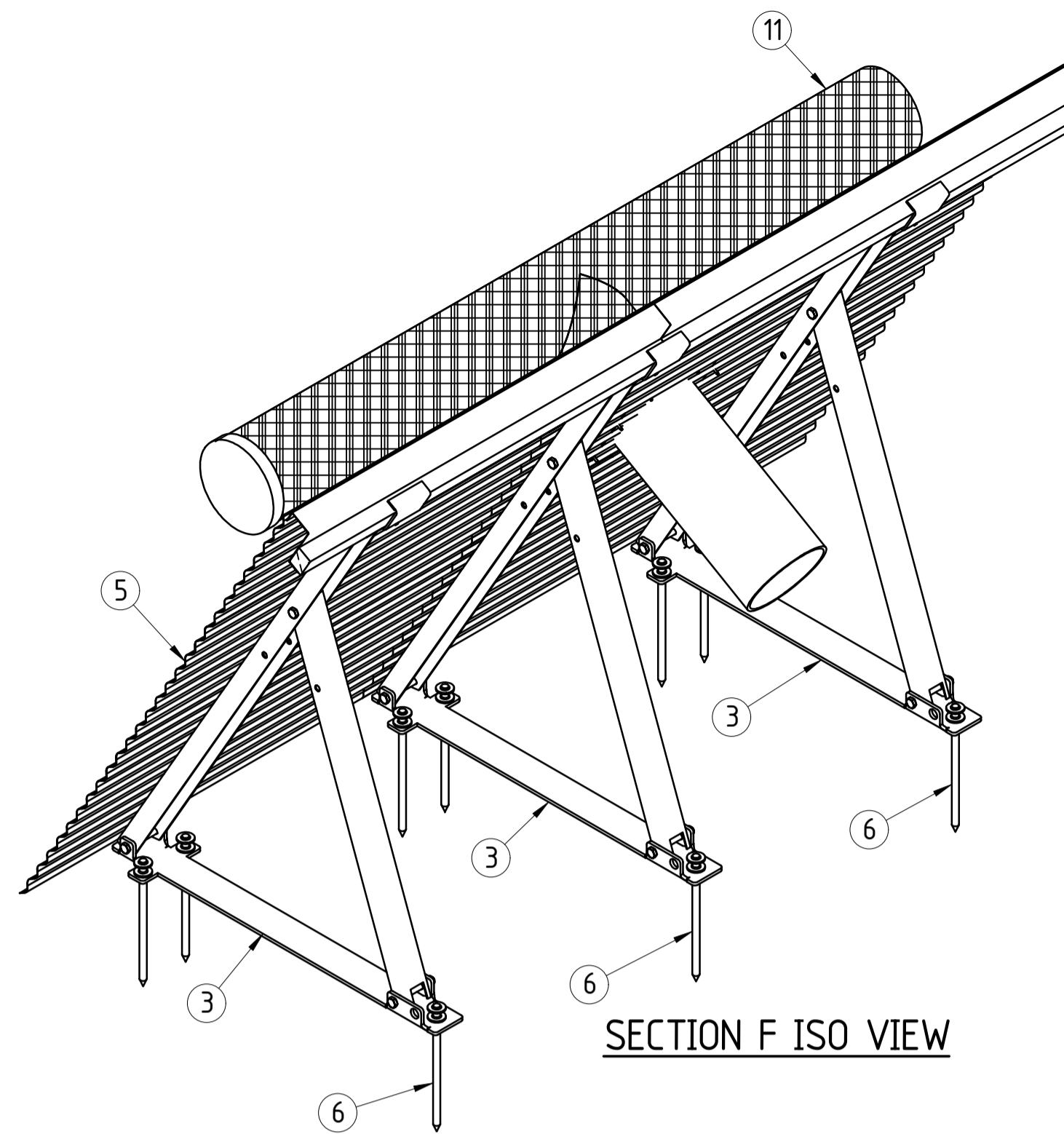




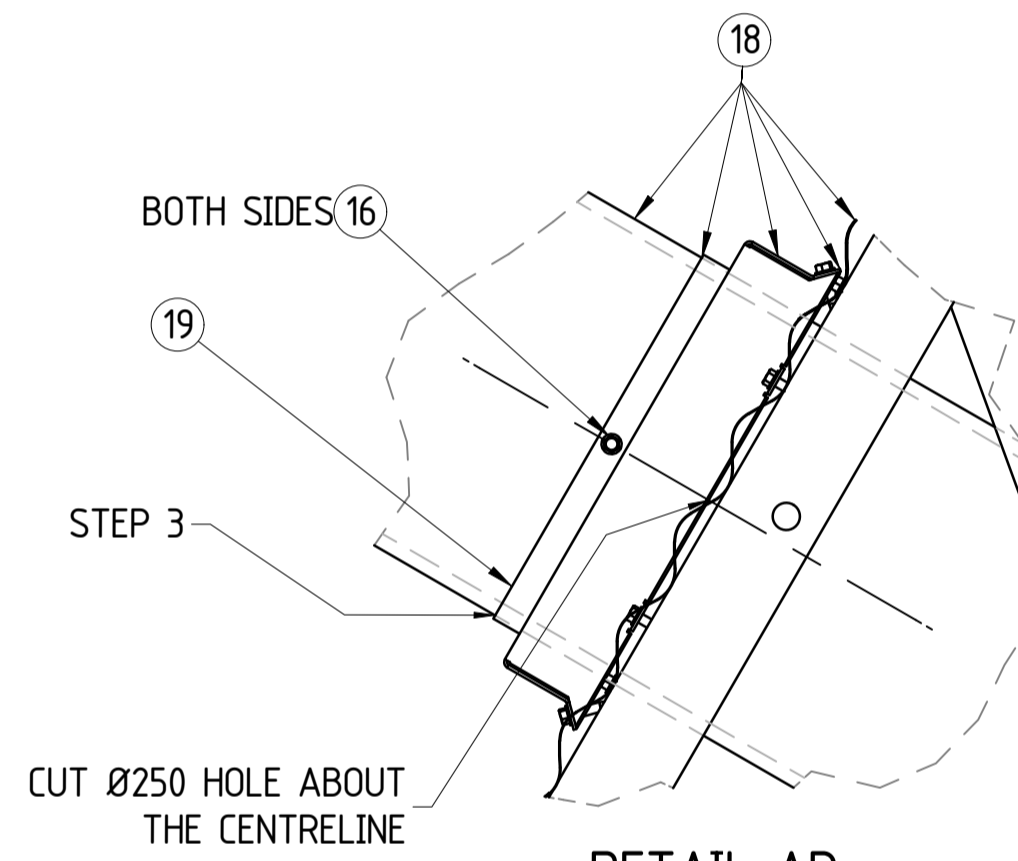
**DETAIL B**  
**'T' PIECE INSTALLATION DETAIL**



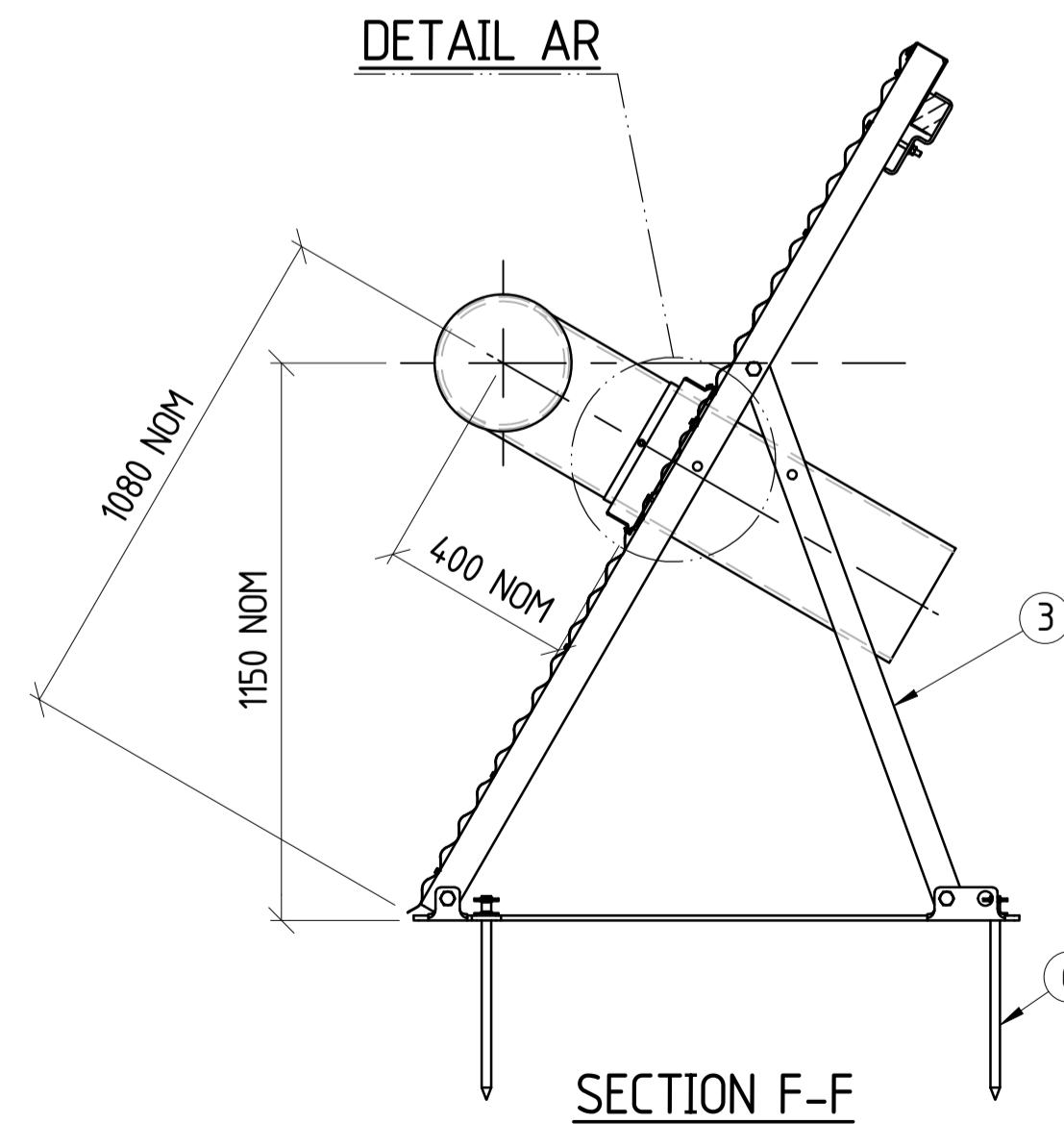
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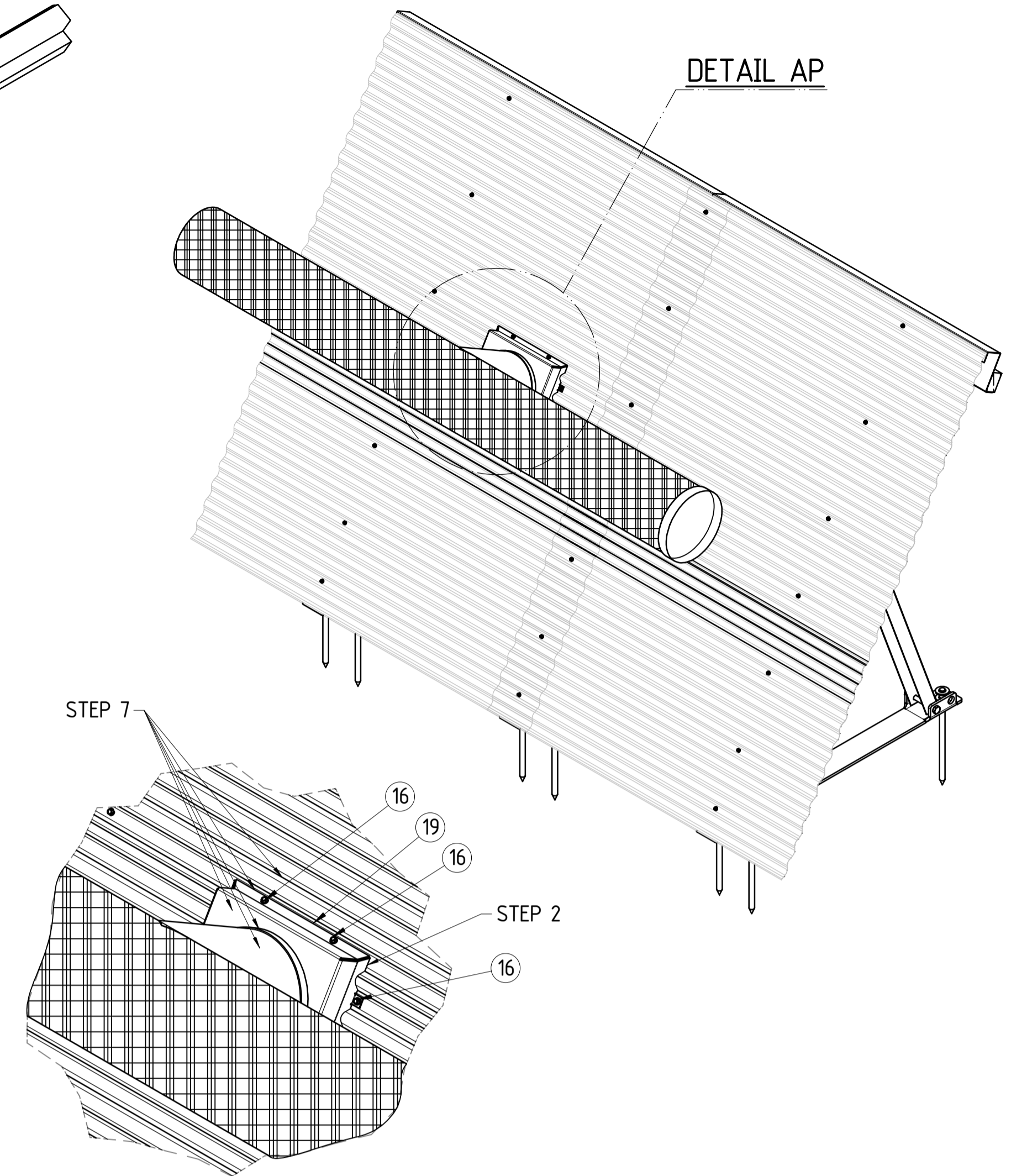
**SECTION F ISO VIEW**



**DETAIL AR**



**SECTION F-F**



**DETAIL AP**

**BACKGROUND:**

- THE 'T' PIECE IS USED TO PUMP HAZARDOUS FUMIGANT INTO THE STORAGE AFTER IT IS FULLY SEALED. CARE MUST BE TAKEN WHEN INSTALLING THE 'T' PIECE AND APPLYING THE SEALANTS TO ENSURE THE SEAL IS APPLIED TO A HIGH QUALITY.

**USE THE FOLLOWING QUANTITY'S PER 1 'T'PIECE**

- 1 X 600ML SAUSAGE OF BOSTIK SEAL AND FLEX 1
- 1L OF NOVALAST LTM 151

**PROCEDURE:**

- STEP 1: PREPARE AND CLEAN SURFACES WHERE SEALANTS ARE TO BE APPLIED AS PER MANUFACTURERS SPECIFICATIONS.
- STEP 2: INITIAL COLLAR INSTALL; APPLY A THICK (5-10MM) BEAD OF 'BOSTIK SEAL N FLEX 1' TO ALL EDGES OF THE T-PIECE MOUNTING COLLAR WHICH WILL CONTACT THE CORRUGATED CLADDING. TEK SCREW T-PIECE MOUNT (WITH BOSTIK SEALANT APPLIED) TO THE CLADDING USING 8 TEK SCREWS, EVENLY SPACED AROUND THE T-PIECE MOUNTING COLLAR.
- STEP 3: INSERT T-PIECE INTO THE T-PIECE MOUNTING COLLAR. SECURE THE T-PIECE AT THE LOCATION SHOWN, USING 2 X TEK SCREWS, THROUGH THE MOUNTING COLLAR RING. APPLY A THICK (5-10MM) BEAD OF 'BOSTIK SEAL N FLEX 1' AROUND THE JOIN AND AROUND ANY GAPS, INCLUDING AROUND THE TEK SCREWS. ALSO APPLY A THICK BEAD TO FILL THE GAP BETWEEN THE CLADDING AND THE T-PIECE, ON THE OUTSIDE OF THE BULKHEAD.
- STEP 4: LET SEALANT DRY AS PER MANUFACTURER'S DIRECTIONS.
- STEP 5: APPLY A SECOND THICK (5-10MM) BEAD OF 'BOSTIK SEAL N FLEX 1' AROUND ALL JOINS BETWEEN THE T-PIECE MOUNTING COLLAR, THE T-PIECE AND THE CLADDING.
- STEP 6: LET SEALANT DRY AS PER MANUFACTURER'S DIRECTIONS.
- STEP 7: PAINT THE ENTIRE T-PIECE MOUNTING COLLAR AND 150MM OF CLADDING AROUND THE COLLAR. ALSO PAINT 150MM OF THE T-PIECE, PAST THE COLLAR RING JOIN. PAINT WITH 'NOVALAST 151 LTM'
- STEP 8: LET SEALANT DRY AS PER MANUFACTURER'S DIRECTIONS.

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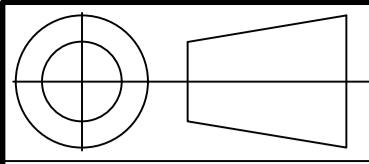
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DRAWN	SCR 10/06/2020
CHECKED	LS 10/06/2020
ENGINEER	BC 11/06/2020
APPROVED	NH 11/06/2020

DRAWING TITLE  
**1.8m OPEN BULK HEAD  
GENERAL ARRANGEMENT  
'T' PIECE DETAIL**

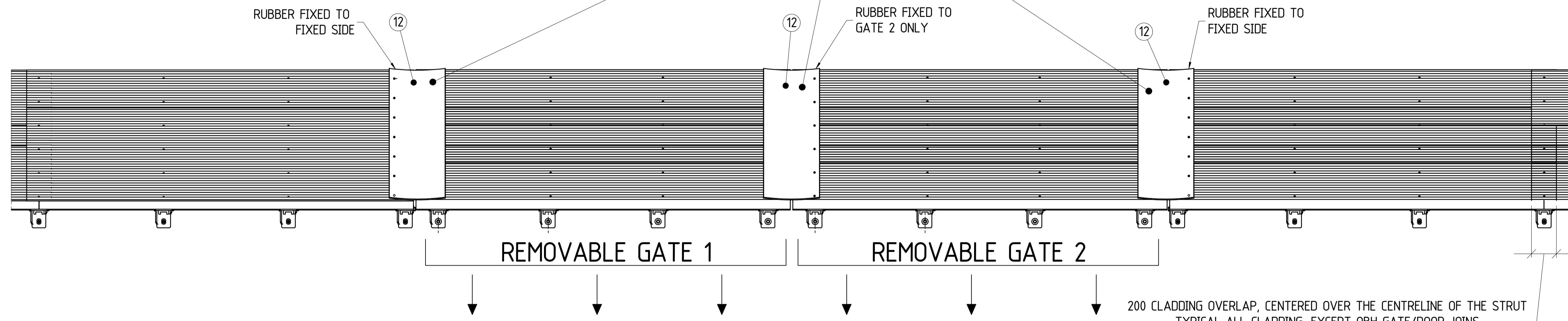
SITE	VARIOUS	SIZE	A1
PROJECT	STANDARD		
DRAWING No	S119-ENG-ST-DGA-0003	SHEET	7 OF 9
REV.	0		





**TYPICAL RUBBER FIXINGS, BOTH ENDS**

- ATTACH CONVEYOR BELT OVER JOINTS (SEE BOM FOR BELT DETAILS)
- FIX RUBBER USING 45MM LONG TEK SCREWS, USING 7 TEK SCREWS PER RUBBER STRIP

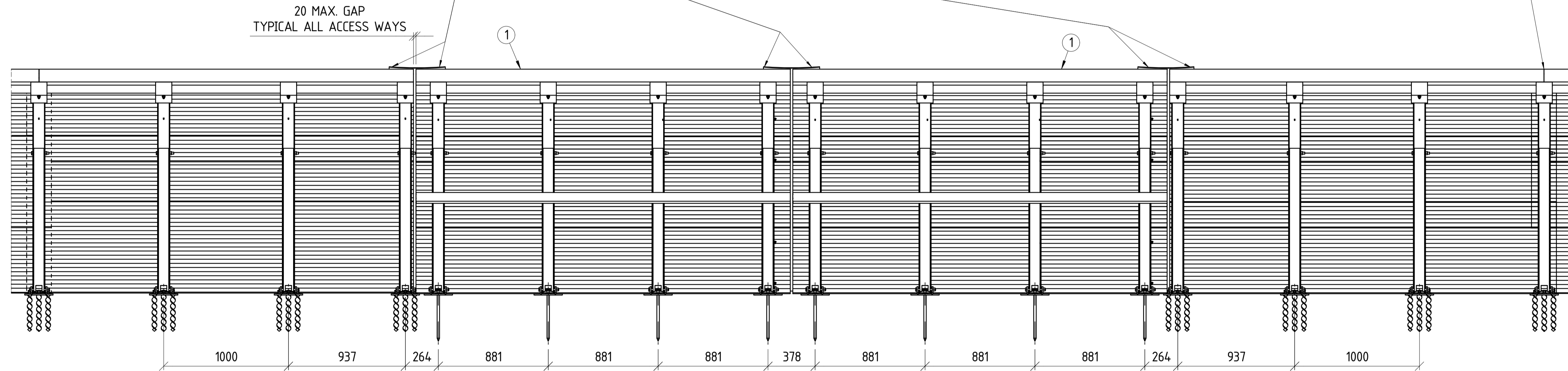


**DETAIL C  
FRONT END LOADER ACCESS GATES IN STRAIGHT WALL  
TYPICAL BOTH SIDES**

CLADDING, Z-CAPPING AND WOOD, ALL TRIMMED TO SUIT ON BOTH SIDES TO CREATE A BUTT JOIN BETWEEN THE FIXED WALL AND THE REMOVABLE GATE. REFER S119-END-ST-ASY-0003 FOR CLADDING TRIMMING DETAIL. MAX ALLOWABLE GAP BETWEEN BUTT JOINTS IS 20MM - TYPICAL ALL GATES

Z-CAPPING MUST BE NEATLY BUTT JOINED AND ALIGNED IN ALL DIRECTIONS TO ENSURE THAT THERE ARE NO SHARP EDGES WHICH COULD PUNCTURE THE OBH TARP. MAX MISALIGNMENT OF 2MM IN ALL DIRECTIONS. TYPICAL ALL JOINTS EXCEPT AT OBH DOOR/GATE JOINTS.

20 MAX. GAP  
TYPICAL ALL ACCESS WAYS



**ELEVATION**

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CBH GROUP HEAD OFFICE  
LEVEL 6, 240 ST GEORGES TERRACE,  
PERTH W.A 6000  
PH (08) 9237 9600 FAX (08) 9322 3942

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		0	10/06/2020	COPIED FROM S-119, ISSUED FOR CONSTRUCTION	SCR	LS	NH							

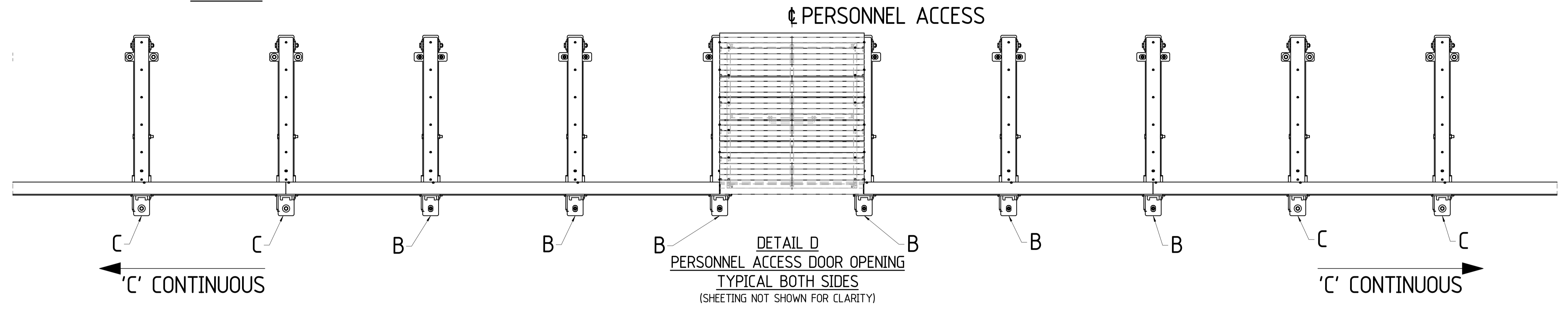
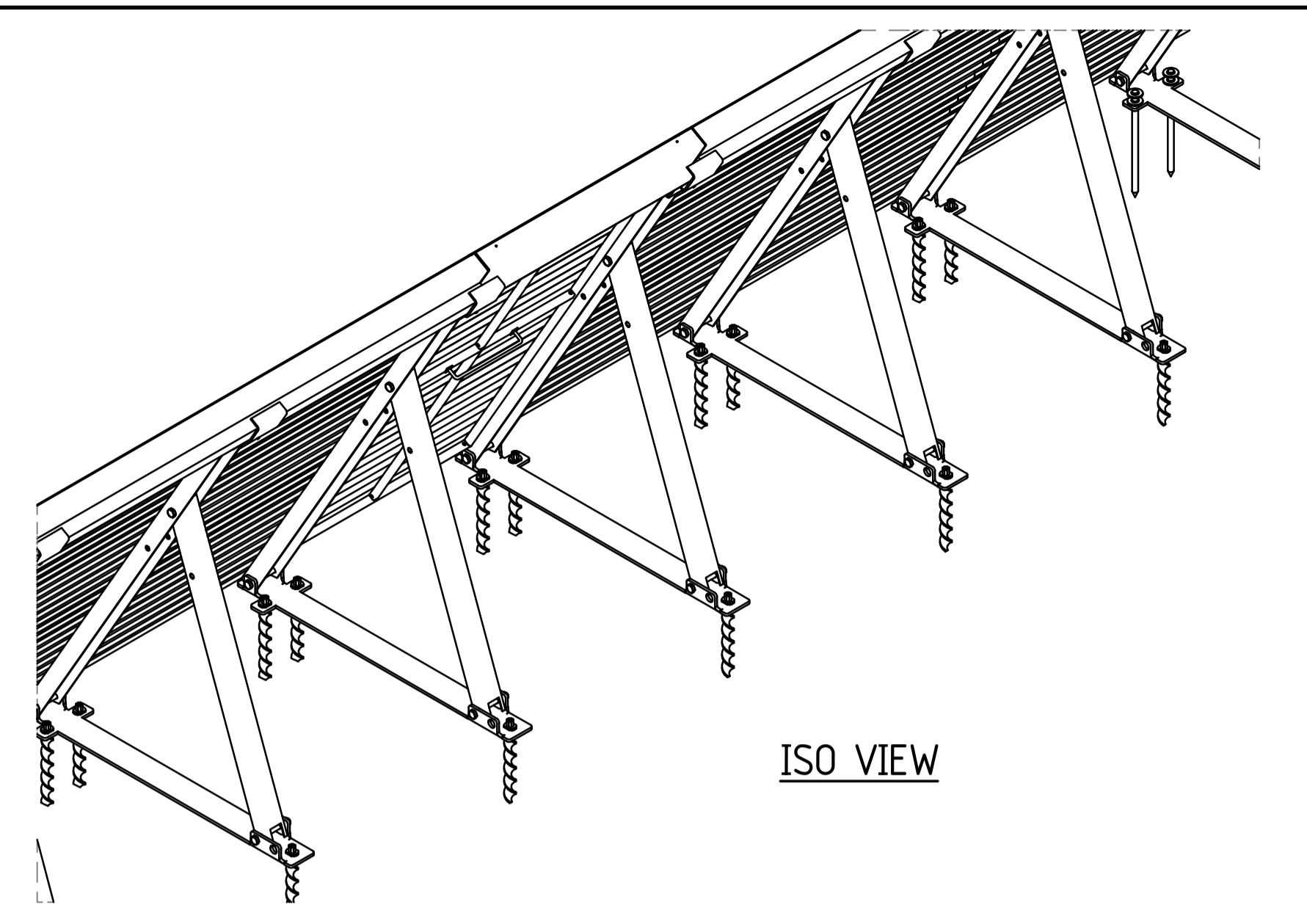
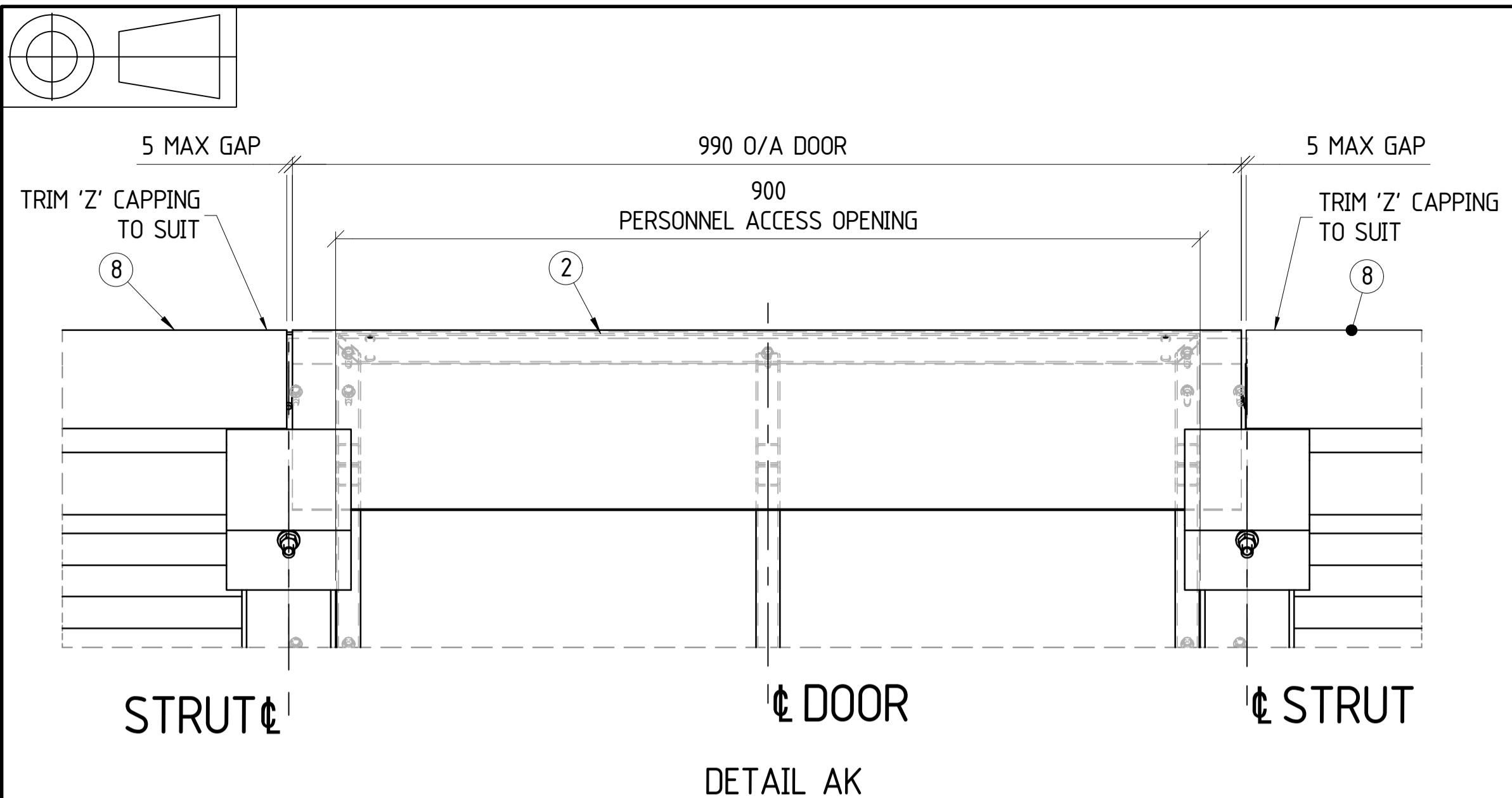
DRAWN SCR 10/06/2020  
CHECKED LS 10/06/2020  
ENGINEER BC 11/06/2020  
APPROVED NH 11/06/2020

DRAWING TITLE  
**1.8m OPEN BULK HEAD  
GENERAL ARRANGEMENT  
STRAIGHT OBH OPENING DETAIL**

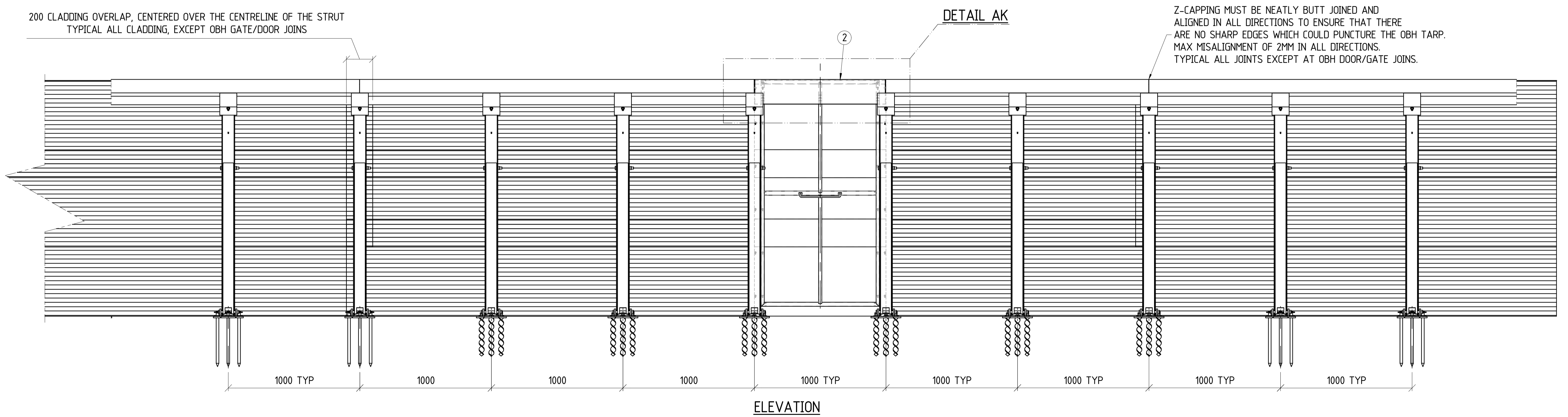
SITE VARIOUS	SIZE A1
PROJECT STANDARD	
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	REV. 0

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200 CLADDING OVERLAP, CENTERED OVER THE CENTRELINE OF THE STRUT  
TYPICAL ALL CLADDING, EXCEPT OBH GATE/DOOR JOINS



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												CHECKED LS 10/06/2020		PROJECT STANDARD												
												ENGINEER BC 11/06/2020		DRAWING No S119-ENG-ST-DGA-0003	SHEET 9 OF 9	REV. 0										
DO NOT SCALE FROM THIS DRAWING										REF DRG No.	REFERENCE DRAWING TITLE	REV	DATE	REVISIONS	BY	CHK	APP	REV	DATE	REVISIONS	BY	CHK	APP	APPROVED NH 11/06/2020		